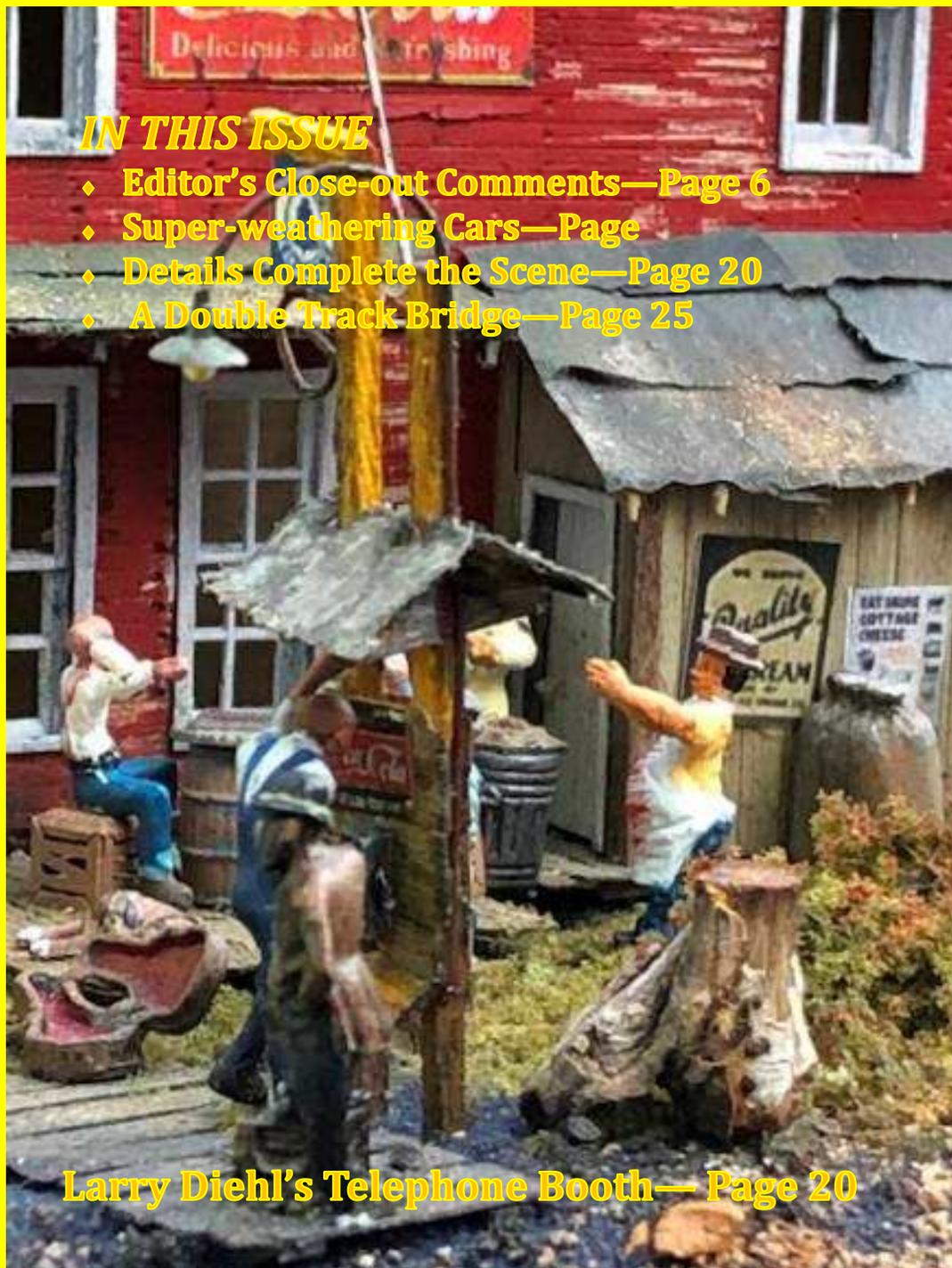


C A B O O S E

K I B I T Z E R



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The Caboose Kibitzer

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Deadline: All items submitted for publication must be received before the dates listed below to be included in the next issue. The Editor will attempt to include the information in the next issue, but there may be a delay because of a backlog.

Content Deadline

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First Quarter	December 1
Second Quarter	March 1
Third Quarter	June 1
Fourth Quarter	September 1

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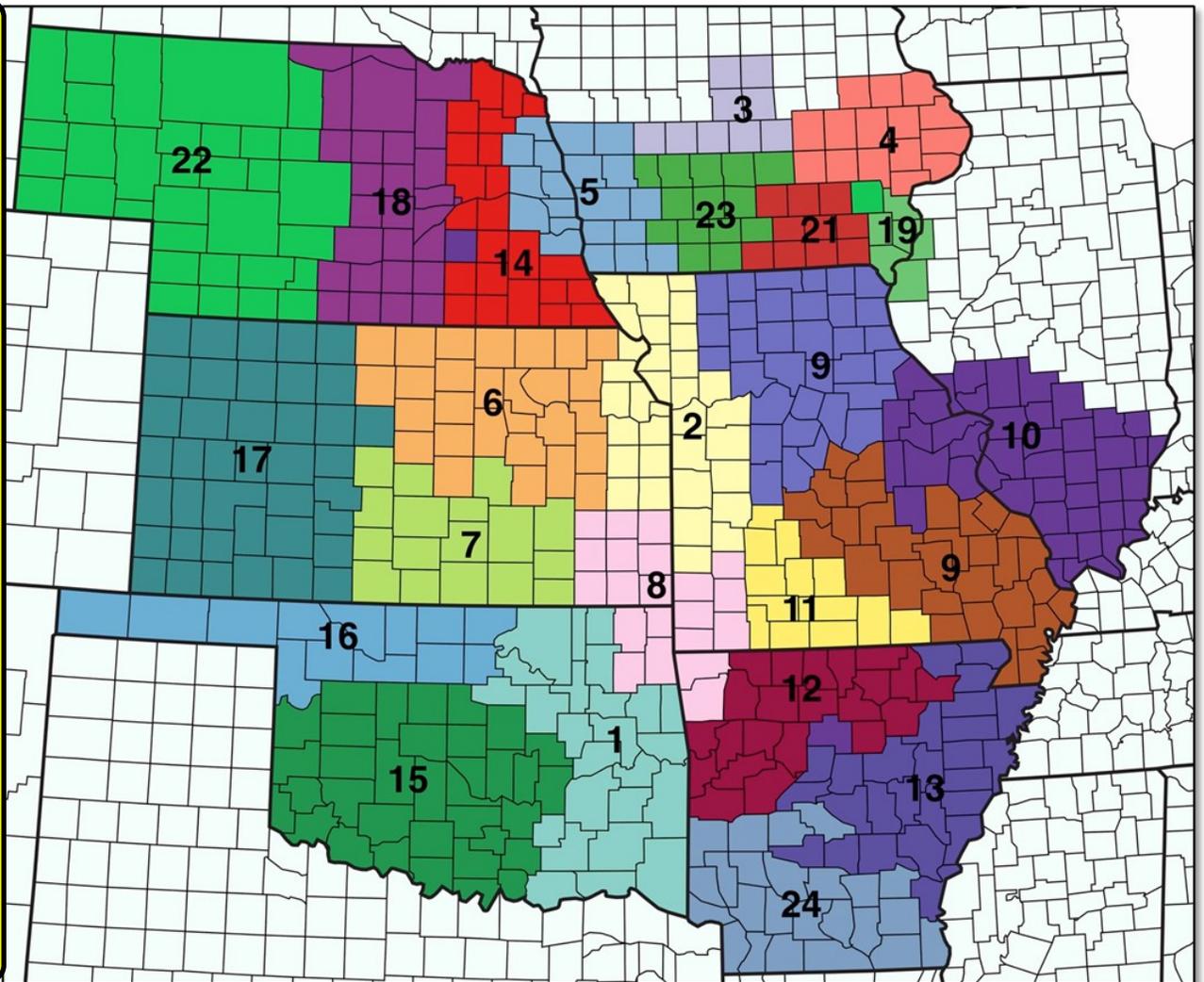
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12	Northern Arkansas Area	Tim Force	dir-2912@mcors-nmra.org				



MCoR NMRA Division Meetings

Note: COVID-19 cancelled many in-person meetings in leu of ZOOM meetings. And now, some Divisions are resuming in-person meetings. Check before showing up...

Division 1: INDIAN NATIONS DIVISION (Tulsa, OK): Meet 4 times a year. Unless otherwise specified, all Indian Nations NMRA meets are held at the new Hardesty Library, 8316 E. 93rd. St. just east of Memorial Rd. in Tulsa, OK. Library opens at 9:00AM and the meetings start at 9:30AM-12:30. Superintendent - Allan Roecker 918-866-5732 or allanroecker@yahoo.com. Web page: www.tulsanmra.org for meeting dates and additional info.

Division 2: TURKEY CREEK DIVISION (Kansas City Area): 4th Tuesday every month at 7:00PM except Dec Holiday Party (Dec 17, 2019), at Lakeview Village, 9000 Park Pl., Eastside Terrace Bldg., Lenexa, KS. For current information: Martin Wade, TurkeyCreekNMRA@gmail.com or the Division Timetable <http://www.tc-nmra.org/TC-Calendar.html>

Division 4: EASTERN IOWA DIVISION: Bob Perkins, director (dir-2904@mcors-nmra.org)

Division 5: WESTERN HERITAGE DIVISION (Omaha, NE/Council Bluffs, IA): 1st Saturday (except January) at 9:00AM in the Sump Memorial Library, 222 N Jefferson St. (2nd & Washington Streets) Papillion, NE (across from Runza). For the latest, up-to-date information visit the WHD web site at:

<http://www.whd.mcor-nmra.org> or the WHD Facebook page, Western Heritage Division, NMRA.

Division 6: KANSAS CENTRAL DIVISION: 1st Saturday every three months. Meetings start at 1PM. For the next meeting's location and program please email joycove@wilsoncom.us.

Division 7: CHISHOLM TRAIL DIVISION (Wichita, KS): 1st Tuesday each month at the Olivet Baptist Church, 3440 West 13th St, Wichita, KS 67203 (13th Street North & High Street). Gathering-6:45PM; NMRA meeting 7:00 - 9:00PM. Info: Dean Lippincott, email dlipp5a@gmail.com

Division 10: GATEWAY DIVISION (ST. Louis, MO) 3rd Monday each month, 7:00PM. Odd numbered months at Trinity Lutheran Church, 14088 Clayton Road at Woods Mill Rd (Hwy 141), Ballwin, MO. Even numbered months at VFW Hall, O'Fallon, IL. Info: <http://www.gatewaynmra.org/division.htm>

Division 14: COWBOY LINE DIVISION (Norfolk, NE): 3rd Thursday each month, 7:00PM at HyVee East upstairs meeting room. Corner 1st Street and Norfolk Ave. Info: Dennis M. Brandt, MMR, phone 402-992-2415, email dennisbrandt44@gmail.com

Division 15: OKLAHOMA HEARTLAND DIVISION (Oklahoma City, OK): meets in the even months in the Oklahoma City area. Contact OHD Director at dir-2915@mcors-nmra.org. All who are interested in Model railroading are welcome.

Division 17: WESTERN KANSAS DIVISION (Dodge City, KS): Meetings pending a new location. Info: Robert Simmons, Division Director, 620-521-3591(C) or 620-272-0444(H). Facebook page "Western Kansas Division"; e-mail: trainman55@hotmail.com

Division 18: PLATTE VALLEY DIVISION (Hastings, Grand Island, and Kearney, NE): Meet quarterly in members homes on a rotating basis or at sites of interest. New members are always welcome. Info: Todd L. Petersen, Division Director, 308-832-2200 or todd@autofuelstc.com



The Head End

By Larry Diehl, President MCoR

MCoR is struggling, with some Divisions and Areas having difficulty with attendance. Therefore, lack of activity grows into deterioration of the Division's administrative structure. Not enough people, no one wants to take on their leadership roles. No leadership resolves into no organized attendance/meetings. This ultimately results in dissolving the Division.

This does no good for many individuals who are wanting to participate in some kind of activities. No place to go. No friendship or camaraderie. Their enthusiasm wanes and their interest for the hobby fades.

Maybe there is a way of intervening whereby these individuals can participate and interact within a group. Maybe they can rekindle their interests and satisfy a desire to learn new things in the hobby. Maybe they renew their spirit of model railroading.

Well, we are discussing the use of a word I dread ... "Zoom attendance".

At Turkey Creek, we have invested in a minimal amount of equipment to make the meetings available on Zoom for the local persons who have problems attending due to work commitment or "out of town" or whatever!

So maybe we invite persons from central Kansas or north Arkansas or central Iowa or western Nebraska to attend our meetings via Zoom. Some issues I heard are it's too far to drive for a short meeting. It's the time necessary to drive, all means I have not enough time after work to go home, eat and make the meeting!

We are in a new era with today's electronics. I'm going to start contacting persons in these distant Areas/Divisions to see if they may have interest in remotely attending Turkey Creek Meetings. Turkey Creek meets monthly and always has clinic presentations on varying subjects. We publish the clinics ahead of time in our Lightning Slinger.

Do we say this is Turkey Creek extensions! We must include them on monthly

newsletters, etc. We must extend greetings and friendships as though they live in our neighborhoods.

Maybe every once in a while, they could come to KC to meet and greet in person, or they make a clinic presentation to the group, or attend model contests, etc.

To all our members, not sure this will work, but I believe it's worth a try. If we can develop a feeling of "belonging" we may retain several persons before they fade away!

MCoR has another issue. We are losing our Caboose Kibitzer editor, Mr. Ray Brady. Ray continues to have health concerns and has moved to Oregon to be close to family for needed medical care and travel responses. We at MCoR have quietly been pursuing persons for replacement, but with no success.

We need a new CK editor. Does anyone have the desire to tackle this activity? The CK is a quarterly publication. This issue of CK is raised last. A new person is needed to start immediately if possible. Anyone with any interest, please contact Larry Diehl, ldiehl066@gmail.com or 816-804-0152, cell phone.

Ray Brady has done a very good job for us the past few years. We will miss him!

2024 Regional Meet... Work is nearing completion to establish location for the 2024 Regional Meet. As soon as possible that we can secure the venue, we will make an announcement.

Thank you for your patience.

P. S, I saw this little guy in an advertisement mailed to me. Thought you all might enjoy the thought as winter is upon us.

Sincerely,
Larry W. Diehl
President, MCoR



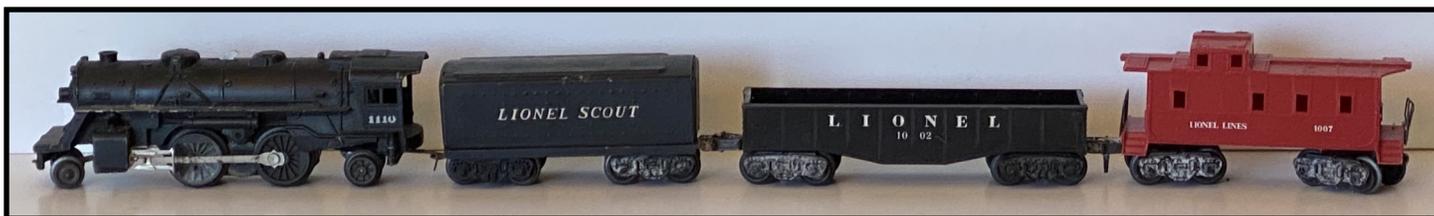


Editor's Musings

By Ray Brady, Editor

This issue of the Caboose Kibitzer is the 16th that I have published, Over these past 4+ years, I have tried to cover items of interest that MCoR members bring to the table. During that time, you have seen glimpses of what I have been doing in model railroading, but I have tried to keep out of the discussions because YOUR stories have so much interest. However, I am entering a new phase of my life that involves a move to Oregon so will have to give up my editorship of the Caboose, It has been an extremely interesting journey on my part, but time moves on and you need a fresh voice. However, before I leave, I would like to offer one last, in depth missive on what my journey in model railroading is about.

My journey in model railroading began as most of you did in the 3rd or 4th grade when I received a Lionel Scout train set for Christmas. I, and my neighbor kid, got ours sets at the same time and we ran trains together in our basements.. I still have that train, as shown in the photo below.



It was much fun, but it was not the precise modeling that we all are about as older folks. You see, we each had an old clunker engine too, and we liked to put them through things that were totally non-prototypical. You see, we would have bridges that we ran across—bridges approximately 4 long OF UNSUPPORTED TRACK. We got to witness the crashes as the engines careened 4' to the floor below. So, yes it was model railroading, but a demolition derby version.

Along came high school, college, and marriage, and model railroading took a back seat to life's other opportunities. College produced an Engineering degree; employment led me to Boeing-Wichita and Pratt & Whitney in Connecticut. And two kids later, we were a happy family of 4. Thirty years later in 1999, I retired to Maine and life was good, up until my wife was killed in an auto accident in 2005.

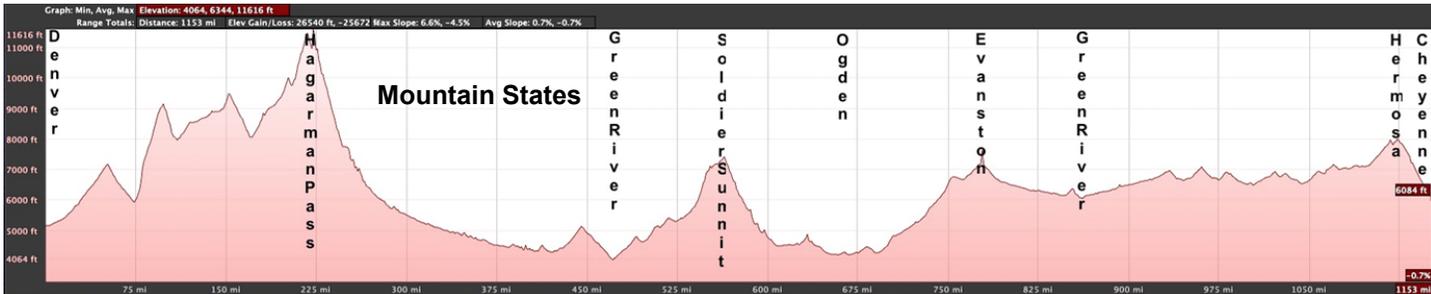
As the result of that accident, I had to find an outlet to work through the grief, so turned to the repressed urge to get into model railroading. In Maine, I had built a barn for workshop, cars, and tractor. But, the loft was 24' X 36' and was an ideal location for a model train. I designed a layout to fit in the loft. The only problem was that I would have to heat it; and family was calling by that time and I decided it was time to move back to Kansas where I grew up. After all, I still had aunts, uncles, and cousins here in Salina. In 2009, I moved back to Salina.

But, throughout all this, the layout of my dreams was gestating. My connection to the railroad was two fold. I had an uncle that had worked his way up from fireman (think coal) to assistant trainmaster out of Omaha on the Union Pacific. And, on our honeymoon, we stayed in a cabin in 11-mile canyon in Colorado. 11-mile canyon road was built on the roadbed of the now-extinct Colorado Midland Rail Road. So I had the genesis for doing a model railroad.

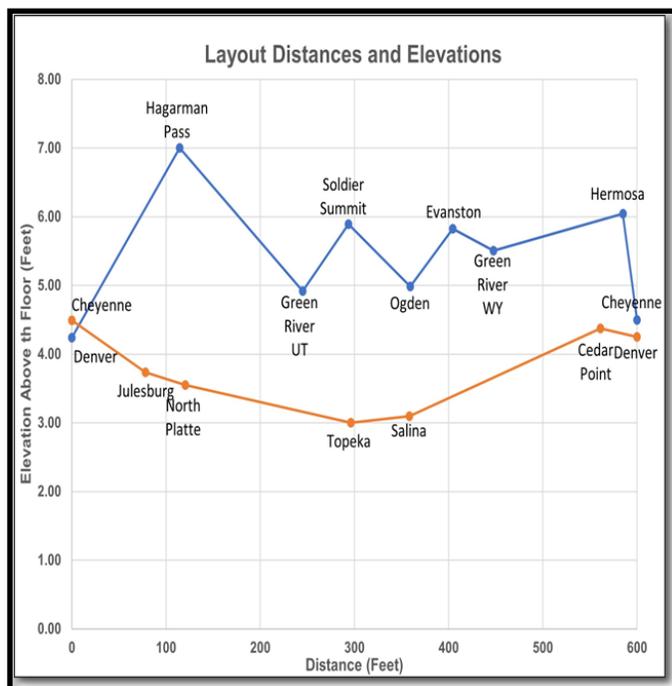
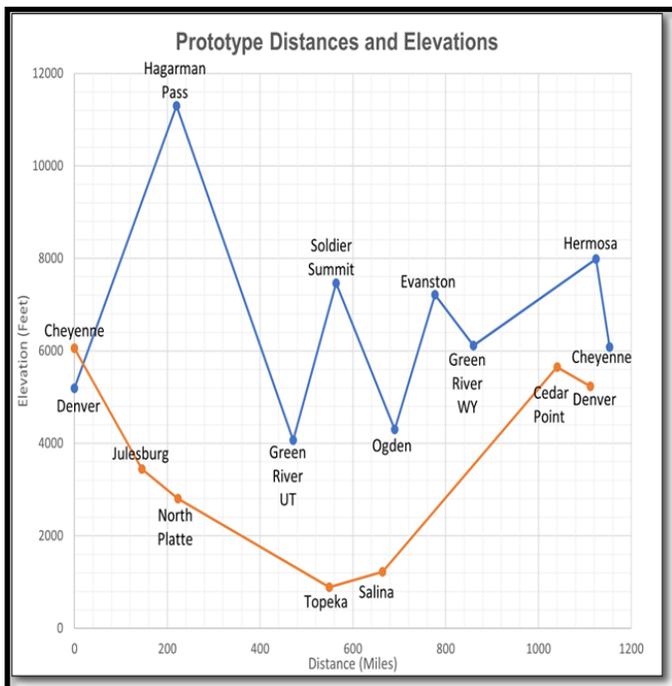
These factors led me to "dream" of a layout that had the attributes of Salina, Denver, Ogden, and then Union Pacific back to Salina. Yes, it would cover quite a bit of prototype territory. But hey, what the heck. Size is only time, and I have plenty of that.

So, I set about designing a layout that would start at Topeka, go through Salina and Denver, transfer to the Colorado Midland to take you through to Glenwood Springs, and then take Denver and Rio Grand Western to Ogden Utah. Then, it would be a simple route to take the Union Pacific back through Cheyenne and North Platte to Topeka. Sounds like a plan. Only 2000 prototype miles and it goes in a full circle. But what the heck. I have the basement in my house all for myself. Big deal.

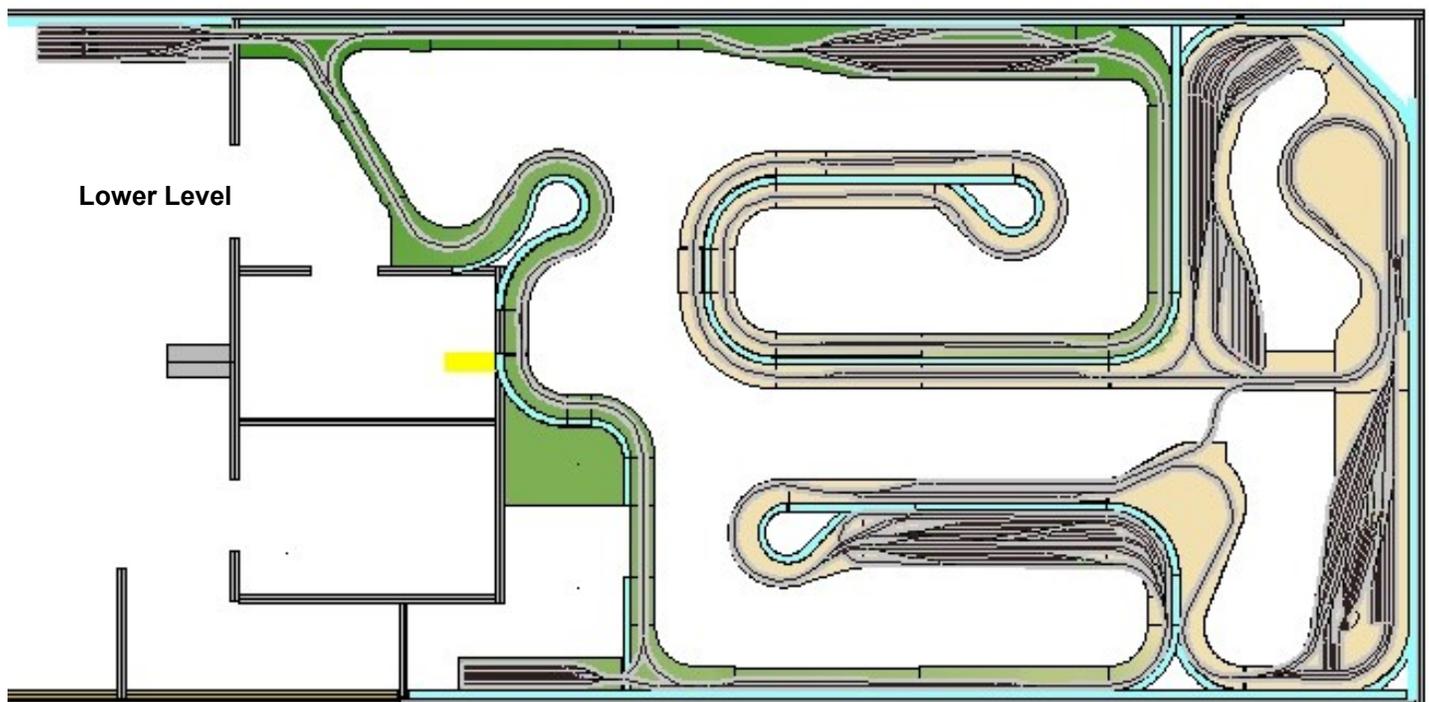
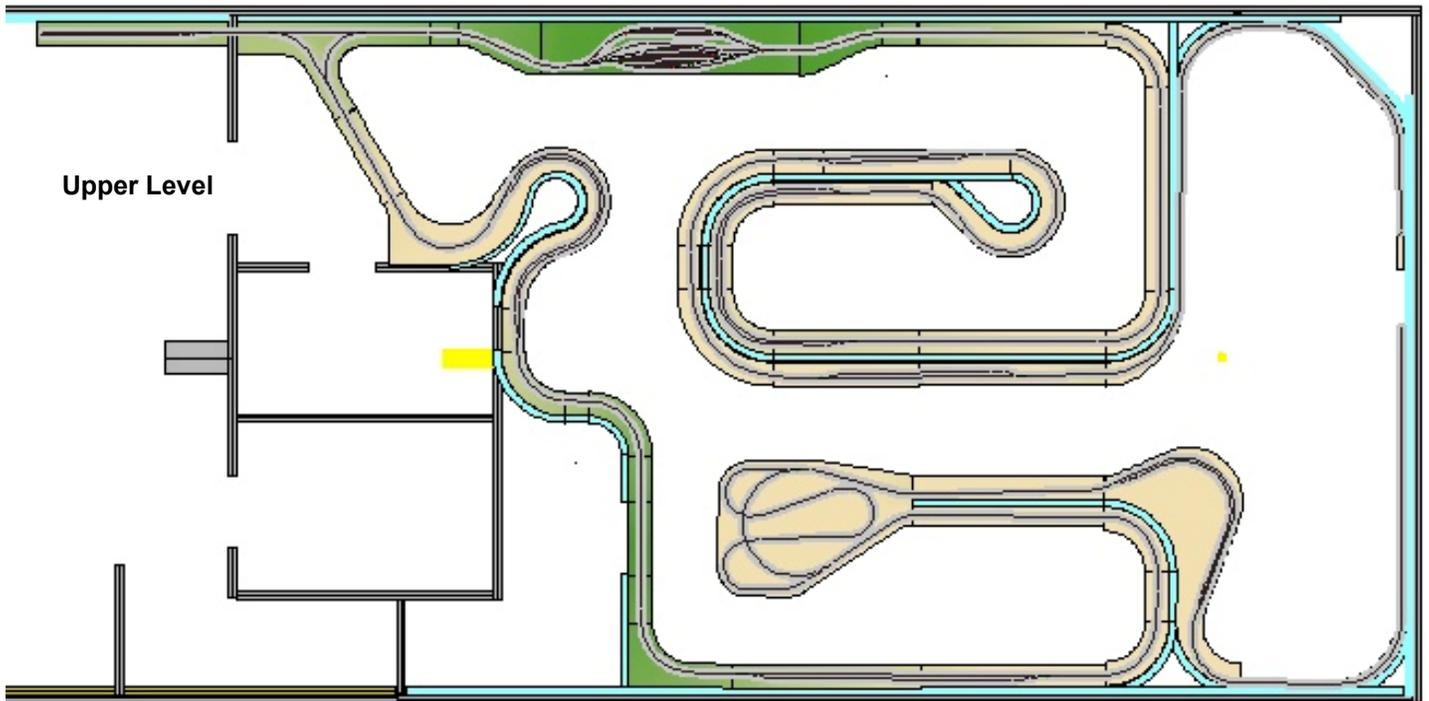
So, I started by defining the geography for this layout. Google Earth came to my rescue, and I was able to produce the following Distance-Elevation charts of the prototype.



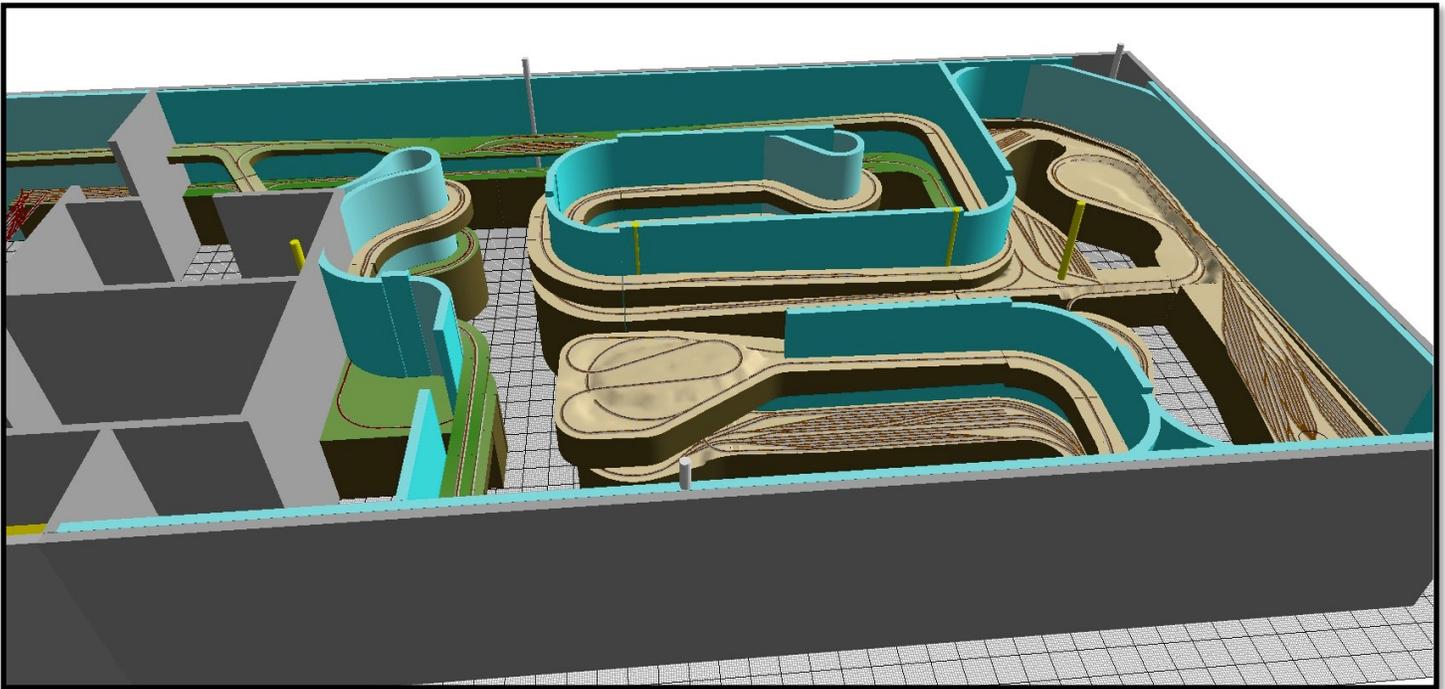
So now, an interesting fact begins to show up, If you superimpose the Mountain States route on the Plains states route with starting point at Denver and end point at Cheyenne, they do not intersect. It is straightforward to have the Mountains state route physically above the Plains state route and they do not interfere or cross each other. The figure below shows the Prototype and Layout Distance-Elevation charts, modified to reflect a simplified model railroad high-low arrangement.



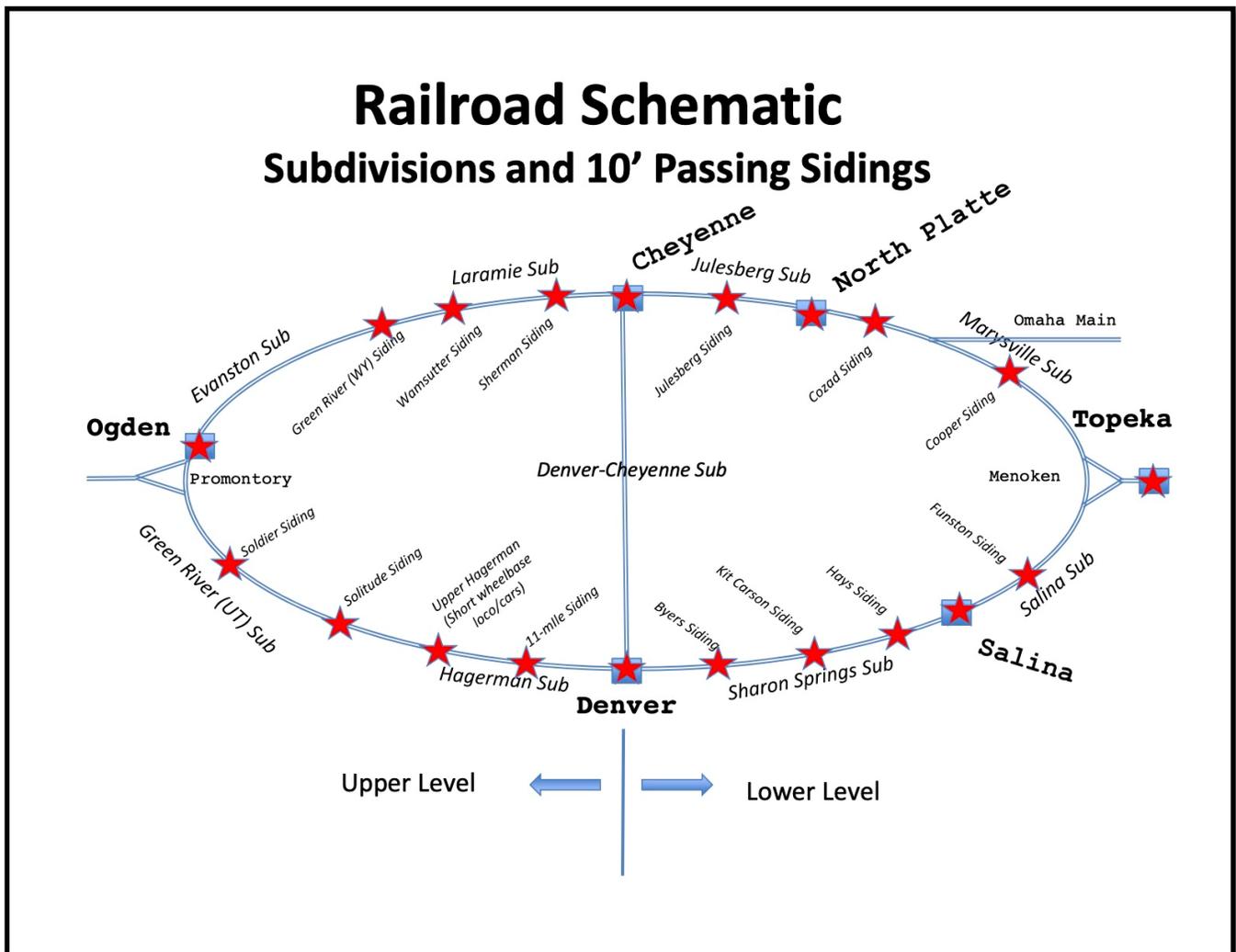
So out of this came a track plan that matched the prototype. Of course I had to scale things distance-wise and elevation-wise, but I was able to give the flavor of the 2000 mile prototype distances in my basement room of 30' X 45'. The result is the schematic of the layout as follows:



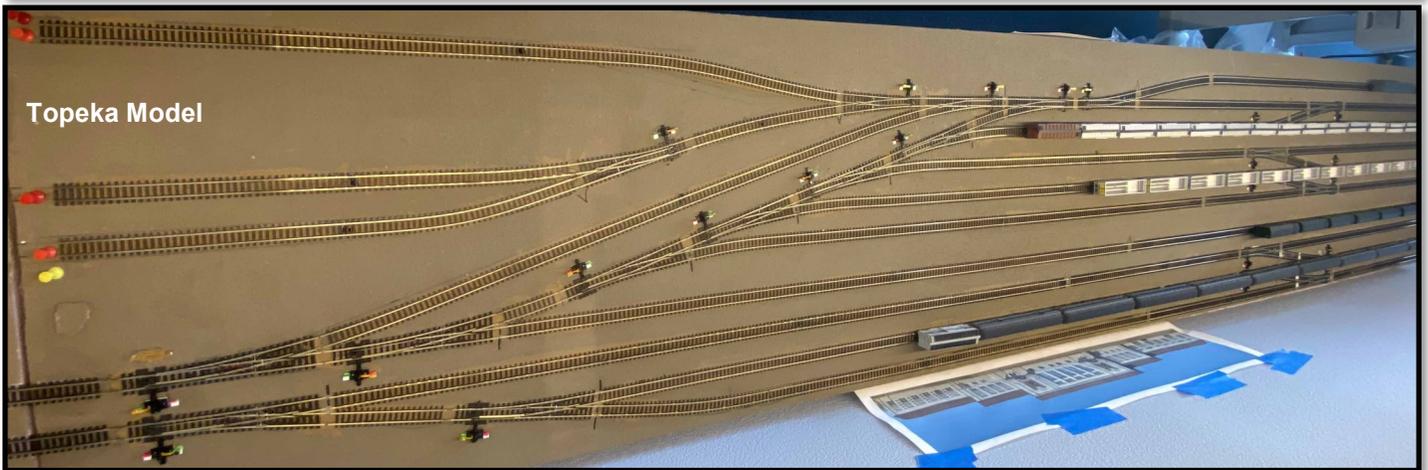
A 3D view of the layout is shown below to get the perspective of the distances and elevations. This layout has a plethora of features. It has the six yards mentioned earlier; Topeka, Salina, Denver,



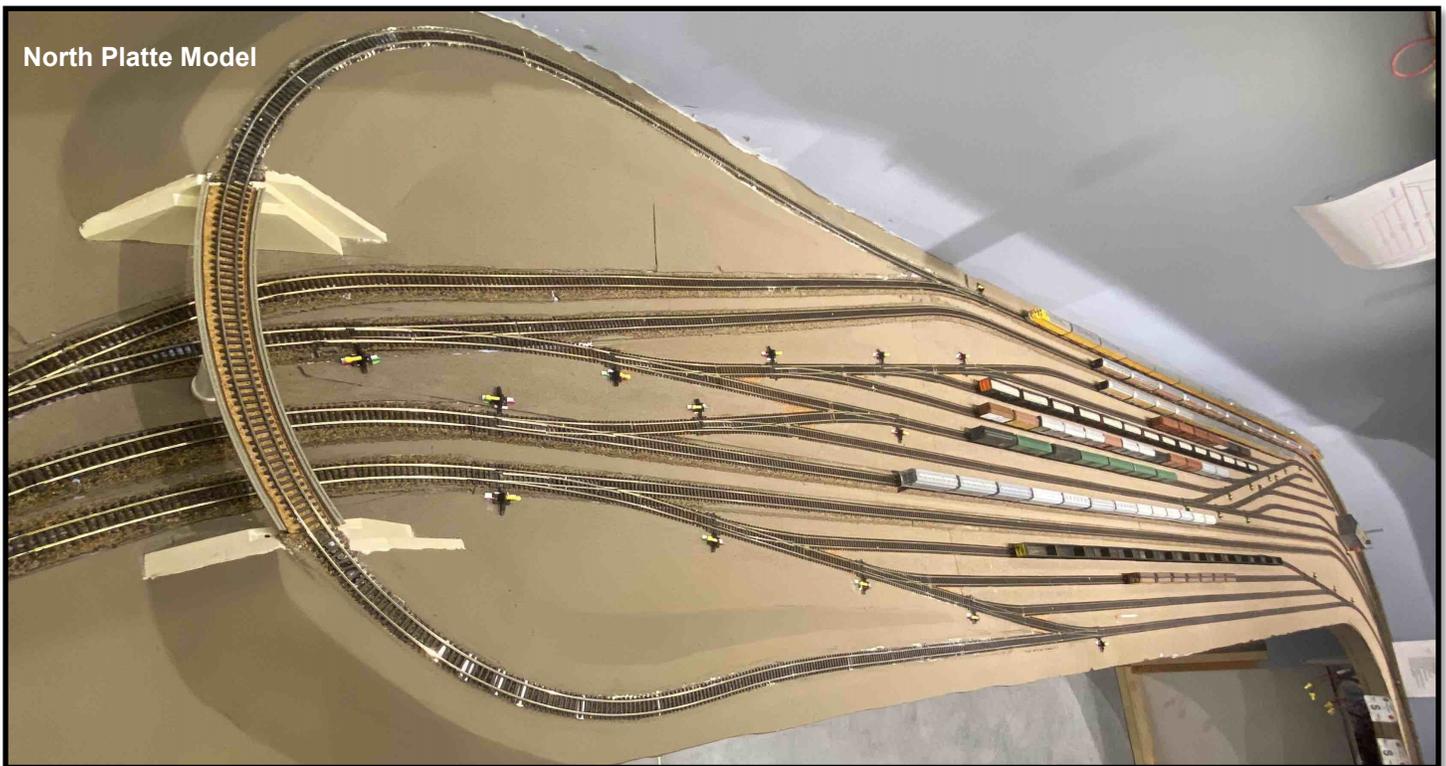
Ogden, Cheyenne, and North Platte. It also has 14 passing sidings each at least 10' long. A schematic of the yards and sidings is show below:



So now the question is how we populate the layout with features. First, the Yards. Based on the real world, there are yards at Topeka, Salina, Denver, Ogden, Cheyenne, and North Platte. Again, taking a look at Google Earth, each of the yards I chose are different configurations. I have attempted to recreate the flavor of these yards as follows:







The Denver and Ogden yards, while not installed yet, will follow the same format of attempting to recreate the flavor of the prototype. When completed, each yard will have enough work to handle one yardmaster, or maybe two, in normal operations mode.

As for the sidings, the list on page 9 tells the story. 14 sidings exist. Each siding is out of sight of the next one, so a Dispatcher is essential to keep the chaos of model running under control.

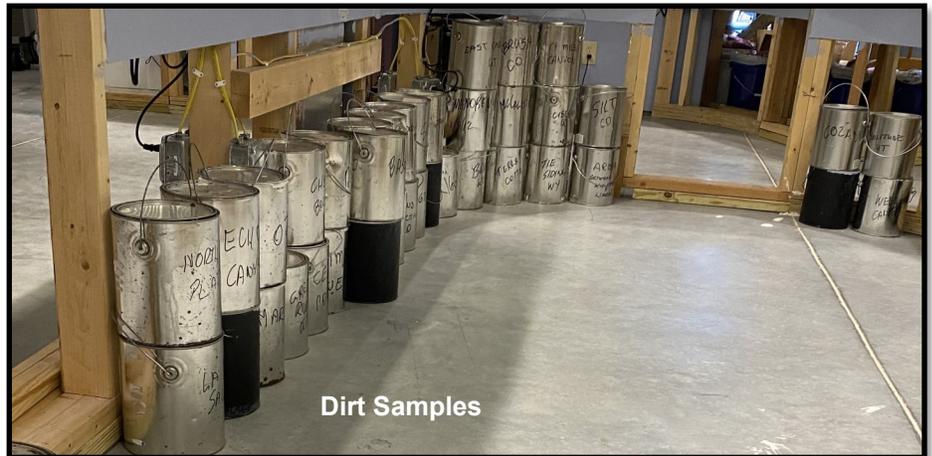
While not spotted, the runs between yards is sizeable. So industries can spring up along the track. Between Topeka and Salina is Ft. Riley. Between Salina and Denver is wheat, ethanol, salt, cattle, etc. Between Denver and Ogden is cattle, coal and other minerals. Between Ogden and Cheyenne is similarly coal, cattle and other minerals. Between Cheyenne and Topeka is grains, beets, and other agricultural products. So industries that would propagate "locals" are abundant.

Couple that with passenger trains such as the City of Denver and the through trains such as the City of Los Angeles, City of San Francisco, or Portland Rose, the possibilities for very complex train operations is obvious. All totaled, there would be jobs for 10-15 people on the layout when completed.

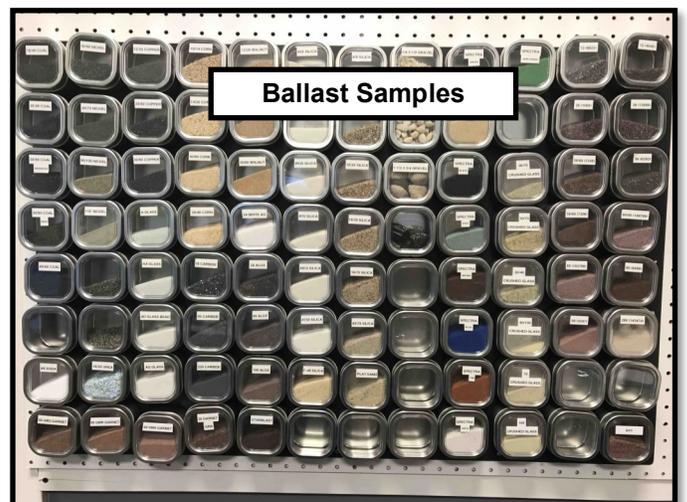
Now, how about the scenery. First the backdrops. I have traversed the entire route of the layout and taken pictures about every 50 miles. Using the individual pictures at each location, I stitched them together using GIMP (freeware) panorama software and the result is what you see here. The following are samples of four locations that I have temporarily placed on the layout. The intent was to have them cover the whole length of the layout, top and bottom sections.



Turning now to the ground cover, having pictures that shows the soil coloration is the start. Along with the pictures, I collected dirt at each location—a gallon at each location. Thus, there will be seamless transitions between backdrop and the surface of the layout. The figure to the right is some of the dirt samples collected.



Then, I got to thinking that the amount of Arizona Rock and Mineral ballast necessary for the layout of my size would bankrupt the coffers. So, looking for alternatives, I turned to United Western of Denver, a supplier of abrasives. The images below and to the right are representative of what they have. And, the cost is \$5—\$10 per 50 pound bag, instead of the much more expensive prices at Arizona Rock and Mineral. And, I attended a Union Pacific Historical Society convention just down the block when I bought my stash. Such a deal.

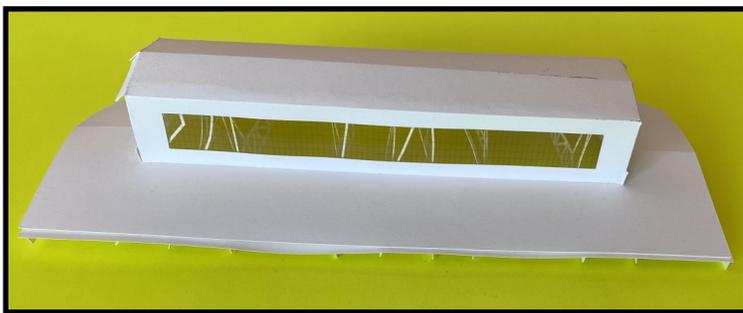
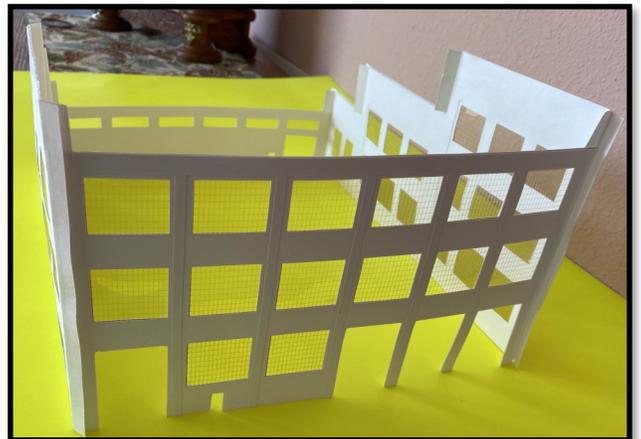


So, there you have it. This is what has kept me occupied for the last 8-10 years. Is the layout done? No! I would have to live another 49 years to complete everything. But, the layout does do a good job of smooth operation. I have had 10 guys down running trains, and while just free-running them, everything ran smoothly. It is a large enough layout that the “dispatcher” can be as simple as each engineer just looking around the corner to see if somebody is headed your way. We have had a lot of fun with just bare track mounted on cork roadbed and glued to foam sub-roadbed. Nothing fancy; just 600 feet of mainline track for which to run. We let your imagination run wild thinking about what the layout represented.

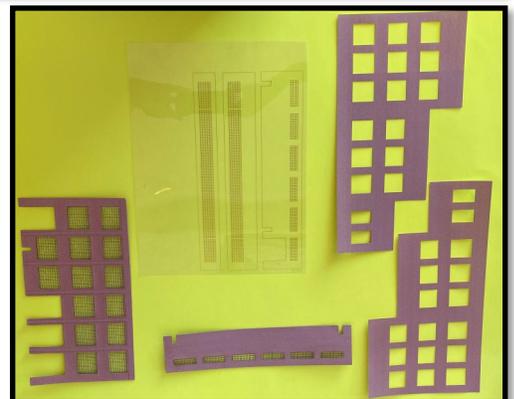
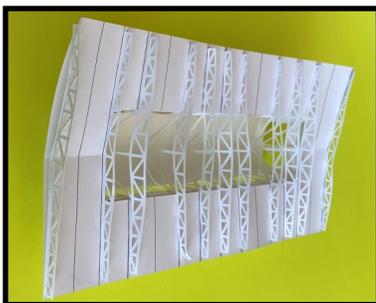
For those of you that are interested, I have a YouTube video that takes you on a virtual tour of the entire layout. The link is found here: <https://youtu.be/jd372wlXqu8?si=ysBLdClbjDP4lrMc>. Settle back and prepare yourself for a 24 minute ride, as that is how long it takes to go around the entire layout at a scale 50 mph.

The Future

But, now that a move to Oregon is in the offing, will my model railroad effort cease.? After all, at 80 years old, I can feel myself slowing down. Well, the answer right now is “NO!” You see, in the last 3 -4 years as I was starting the Cheyenne yard, I chance encountered an N-scale replica of the 600-ton coaling tower that Union Pacific used to coal their steam engines in Cheyenne. I HAD to HAVE IT! But that also meant that all the other buildings at Cheyenne would need to be prototypical. So, in looking around, I stumbled upon a Cricut machine, a machine that cuts cardstock, styrene, or wood, as well as uses pens to mark the sheets.. Investigating, it could be used to make walls in the shape of prototype buildings. WALLA! Making the engine backhouse (used by the steam



program), the roundhouse, and the Diesel facility in Cheyenne became a reality. Thus, I encountered a major shift in the realism that I could put in the facilities in Cheyenne. I GOT DIVERTED. The figure to the right is the Cricut machine, and the figures below are a test sample of the Backhouse that I have started for the steam program. It is complete with walls, roof, and trusses that are representative of the



real world. Using cardstock printed with images from Textures.com, it will cut cardstock, styrene, and wood. And, it will mark window panes on clear sheets of plastic. It is clear you are LIMITED ONLY BY YOUR MAGINATION.

And, about the same time as I got started on cardstock buildings, I became aware of some work people were doing in 3D printing of engines and rolling stock. Items became available on the internet

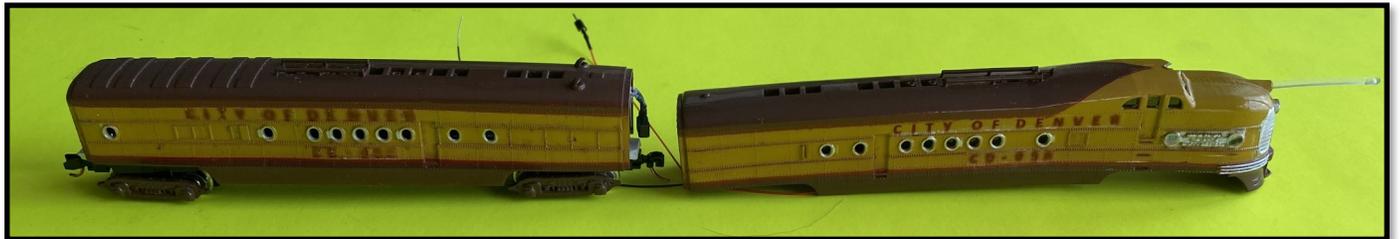


that were not available from the normal engine manufacturers. These were::

- Union Pacific’s M10004 City of Los Angeles and City of San Francisco engines



- Union Pacific’s M-10005 “City of Denver” engines



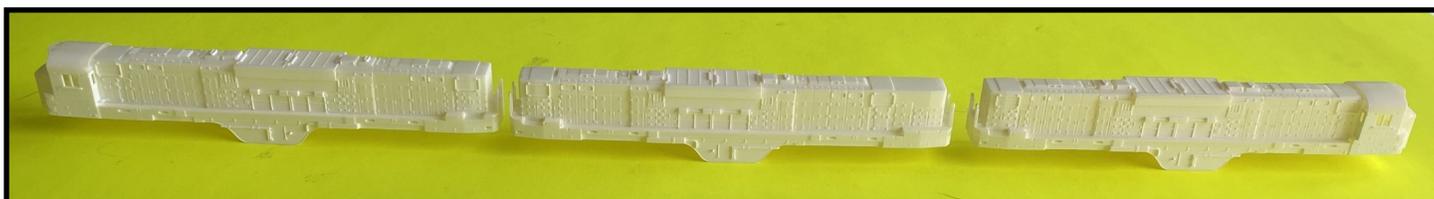
- Union Pacific’s M 10006/7 “City of Los Angeles” and “City of San Francisco” engines



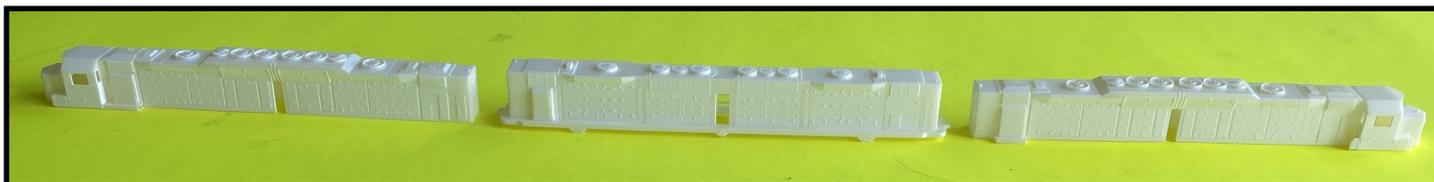
- And, of course each of the ‘City’ trains have cars that were 3D printed.



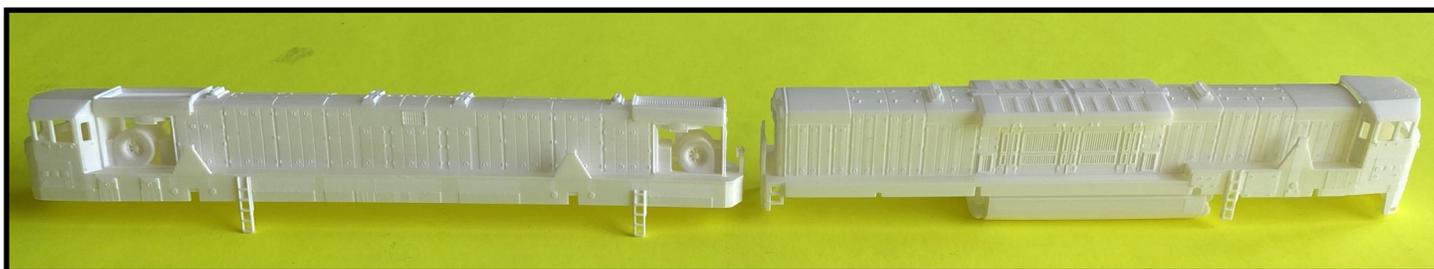
- Union Pacific’s EMD DD35 double diesel in A-B-A consist. These were predecessors to the DD40.



- Union Pacific’s ALCO 855 double diesel in A-B-A consist. Only one set was manufactured.



- Union Pacific’s GE U50 and U50c double diesel.



Since all of these engines and cars are just bare 3D printed, the whole process of painting, lettering, lighting, and powering must be done. Sounds like a good job for someone that cannot spend time standing around working on a layout. Humm! I think I have a plan.

So, the move to Oregon, as emotional as it may be, has a redeeming factor. I can happily work on trains in my apartment and maybe even start a SMALL layout.



A Final Touch For a Weathered Car

By Robert Simmons

Recently, the Purchasing Department of the Western Kansas Rails N-scale layout acquired the four-car set of Union Pacific grain hoppers that come “weathered” from Micro-Trains. The four cars are individually weathered, some more than others, and are nicely done in all respects. The WKR Car Department had only three minor issues. First is the WKR requirement of metal wheel sets. Second, the coupler boxes are not the car body color. And third, the color choice for weathering the underside, all issues easily fixed.

The main purpose in the purchase of these weathered cars is to view their weathering efforts and try to determine the techniques used so we can begin weathering the fleet of older grain hopper currently serving the WKR.



The Paint Shop has acquired over the years a small collection of craft paints. We put a smear of the actual paint on the lid of the bottle to aid in spotting the correct colors when looking through the cabinet. Sometimes we get lucky with a perfect color match right out of the bottle, or we can mix the correct color. We evaluate the paint match with a tiny dab of new paint on the

underside of the car.

To match their weathering effects, you need to begin at the beginning, in this case the main body color. You need to select your color in good light and preferably from the car itself. We picked a car that had a portion that was not weathered.

Since the WKR does not use magnetic uncoupling, the first item of business is to use a needle-nose pliers to push the magnetic pin down, so it is flush with the top of the coupler, then we use a pair of Xuron cutters to trim the pin as on the truck on the left. This gives a better look and prevents the magnetic pin from fouling on switches or grade crossings.

For several years now, the WKR has not allowed plastic wheels, so the next step is to remove the plastic units and throw them away. The metal wheels have half the rolling resistance so more weight can be added to each car over-and-above the NMRA weight standards. We have found the heavier cars track much better, perform like the real cars, and derail less often. Since there are no grades on the WKR, we go almost twice the NMRA standards, however if your layout features grades, you may not wish to add weight.

After trying many assorted brands of replacement metal wheel sets, we settled on Fox Valley sets, but they developed production issues and became almost impossible to acquire, so we tried the Eastern Seaboard Models (ESM) units and made these the new standard. On some of the previous brands we had as much as a ten-percent failure rate but with ESM we have found only a single axle





with an issue.

The Micro-Trains weathered cars come with the standard, black, trucks with some weathering. The black trucks, coupler boxes and couplers just seem to disappear. Many brands now feature body mounted couplers with boxes the same color as the car body and look realistic. Body mounted couplers (because of their limited coupler swing) have a disadvantage of layouts with tight radius turns, (the WKR has one such turn). So Micro-Trains trucks perform better in these cases. We can get the best of both by painting the coupler boxes the car body color.



In your local grocery store you can probably find similar wood or bamboo skewers. This pack is twelve-inch skewers, so we cut them in half and used a pencil sharpener to add a point to the other half. These are excellent for handling the Micro-Trains trucks while painting. Simply insert the pointed end into the bolster hole of the truck, slide up and gently twist to lock in place.



Another trick of the Paint Shop is using old plastic bags from purchased items as an artist pallet. You put a small dab of paint out of the bottle, or more dabs to mix your custom color. This view shows some of the custom colors we mixed to match cars.

In recent months, the Paint Shop took scores of older cars that had been re-weighted and had new trucks with ESM wheels added. We were quite successful in mixing craft paints to perfectly match the body colors for a much better look. It so happened that in some cases we found a perfect color match right out of the bottle, we took the trucks mounted on skewers and painted the coupler boxes the base grey color to match the original body color.



Since Micro-Trains used an airbrush to weather the underside grime, we discarded the idea of doing the same, and instead chose to use weathering powders for such a small area. In going through our stock of AIM weathering powders, we selected "Dark Earth" as the best possible match. To the left is the color we would normally use for Western Kansas weathering. Although the darker color is perfect for most of the country, Western Kansas is prairie land on the edge of the High Plains Desert, so the soil is more clay and when dry is mostly a light tan color.

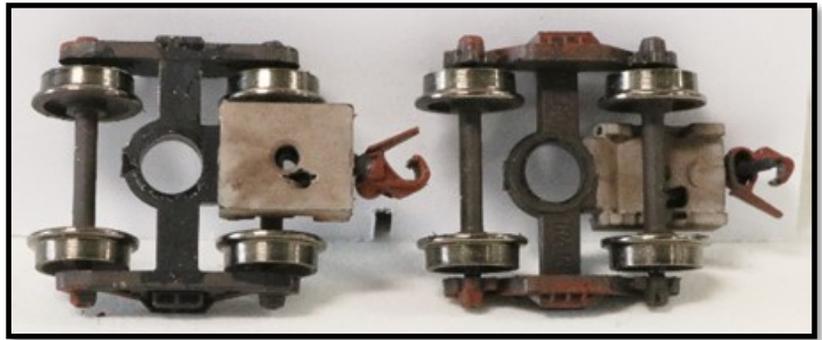
A tip from the Weathering Department is when purchasing frozen meals, wash and store the plastic containers. When applying weathering powders, do so while holding the car over a clean tray to catch any spillage or excess powder to place back in the container. We use stiff bristled brushes to apply the powders and keep them separate from paint brushes. The Dark Earth color was applied over the grey paint on the coupler boxes and was a perfect match to the weathering paint color.



Model Master "Rust" color is used to lightly apply "rust" to the truck side frames. If you use a dry brush method (where you wipe most of the paint off on a paper towel) and lightly brush over the truck outsides, it will highlight the truck details. This same color is applied to the couplers.



The finishing touch is to paint the metal wheel sets. We chose Micro Lux "Railroad Tie Brown" to simulate the dark rust color on older wheel sets. We install the wheel sets in the trucks, then using the



skewer, place the truck on its side near the edge of your table. Once you have the paint on the small brush, hold the truck with your fingers while turning the bottom wheel with your thumb to get a good, complete coat on the outside of each wheel. Then we also paint the backs of the wheels and axles as well.

Now that all the pieces are completed, here is a top and bottom view of the finished trucks ready for installation.

With the trucks reinstalled on the car body, you can see the significant difference with the coupler box looking like it is part of the car body with the shiny wheel treads you can only get with metal wheels. You also get the enhanced coupler swing from truck mounted couplers.

The new "old" cars look right at home on the Western





Details ...Complete the Scene

By Larry Diehl

Looking for an interesting detail, consider an “Old Time Public Telephone Stand” It can be a “people attractor”. It can stand alone outside a business. It can compliment a station scene or a sidewalk in a business setting!



So, a little history will enlighten your thinking. The very first public pay phone occurred in Cincinnati in 1879. The telephone came to Kansas City in 1887 with the first “directory” being one typed page. The numbers were all two-digit addresses. All calls went through a manual “plug and talk” switchboard.

An early telephone stand can be detailed as “beehive of activity” as in my Chillery’s Café & Restaurant diorama, or it can be a visual anchor point outside a structure as in my Icing Platform diorama. I have used this detail on at least a dozen of my models. Somebody always seems to find it, announcing “lookie here”! The old telephone seems to trigger our memories as telephones “as you

recall". Telephones aren't seen on model scenes every day!

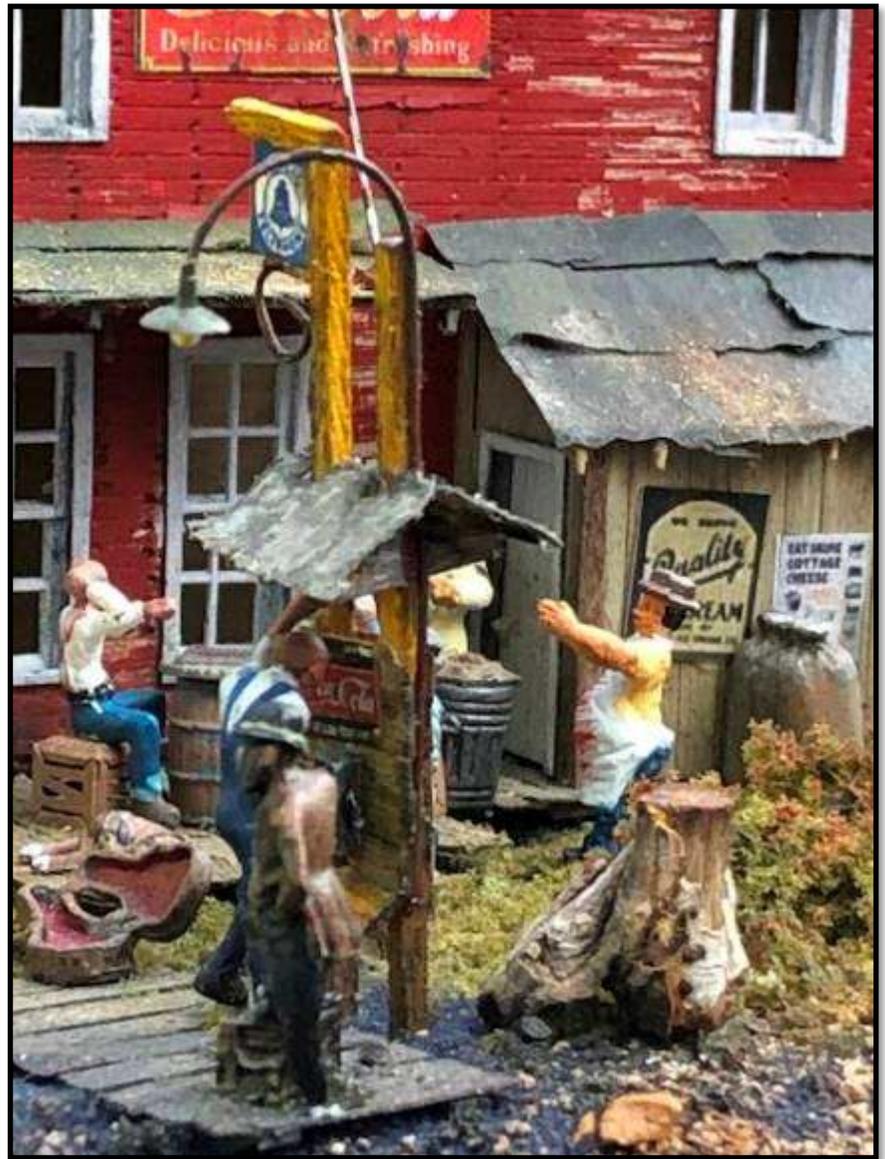
There were times when you first picked up the hand piece, you dialed or got an "operator" - a real person! Usually a "she." She manually routed your call through a switchboard to another exchange or to a long-distance operator, all as identified in the numbers of your call address, i.e. CY9-8021 or FI-2-8021. In the beginning exchanges were named: Cypress, Finley, Drexel, Atlantic, Niagara as examples. Remember those days!

The secret weapon in my fictitious community of Black Rock was the "operator". Most of the time, she was helpful, usually spoke with a soft voice, and the guys could not

restrain themselves, trying to strike up small talk, flirting! Such was the case at my "Black Rock". I'm sure I know who the operator was!



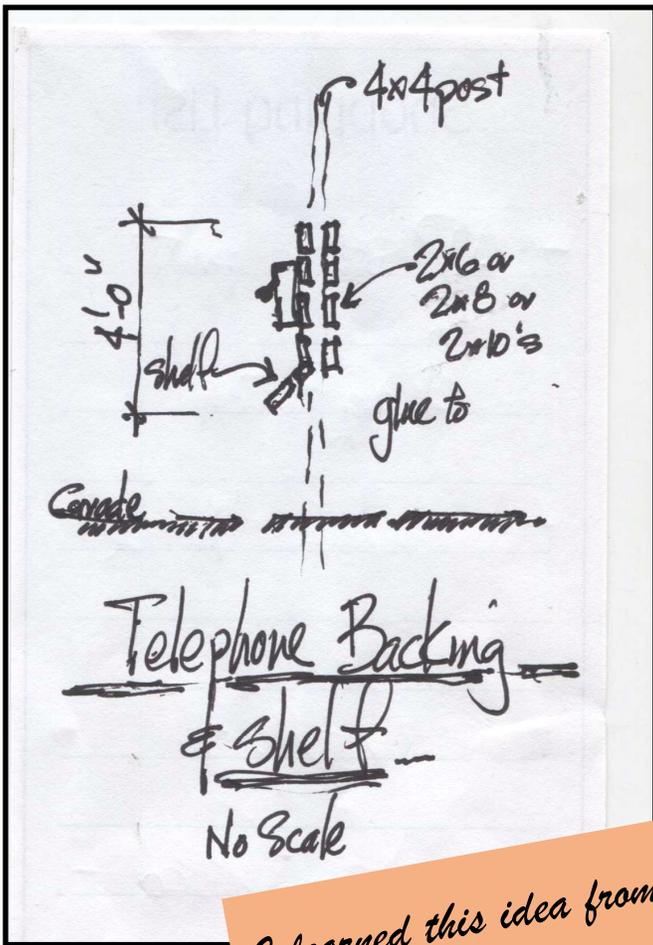
I believe her name was Marilyn! And I think she liked the attention!



Scratchbuild this simple

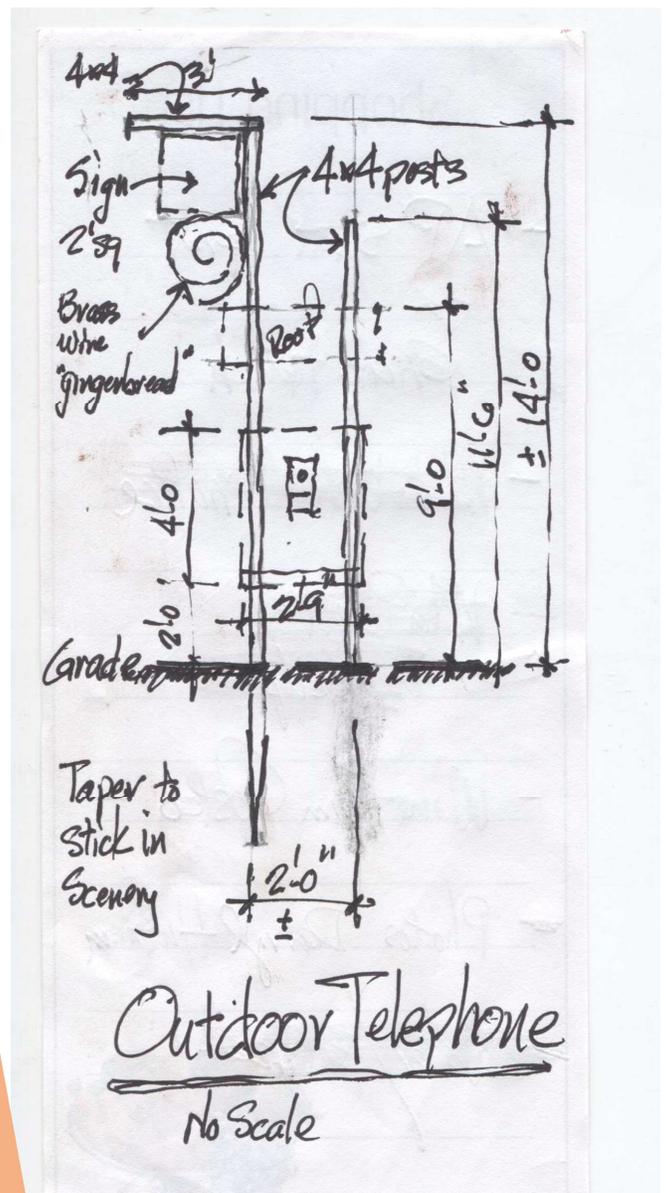
- + Start with 2-4x4 posts.
- + Then add 3 to 4 - 2x6's to make a base to receive the phone, front and back.
- + SS Ltd. "wall phone" #2245.

Make one about 1/2" long to "plant" into a hole in scenery.

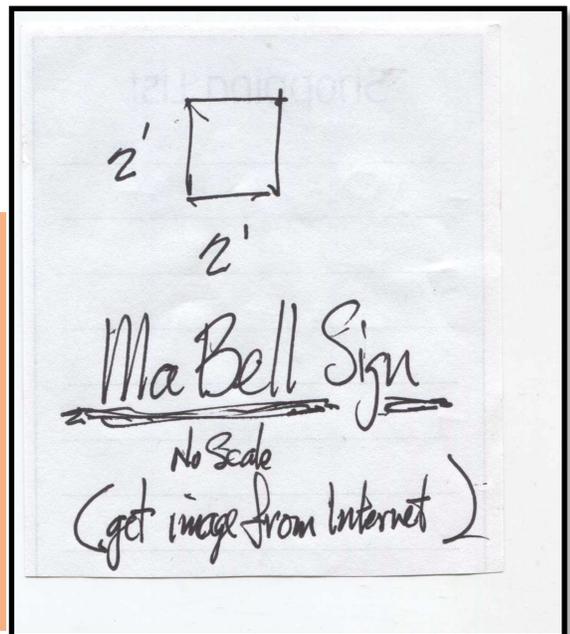


I learned this idea from Doug Fascale at an east coast conference... then I modified it a bit!

Larry Diehl



- Sign from internet.
- + (#0) make about 1/4" - 5/16" sq.
 - + (O) make about 3/8" - 1/2" sq.
 - + Glue on each side of a piece of cardstock ... to stiffen, suggest 5 min epoxy.
 - + Hang on 4x4 post with a horizontal 4x4 support



The Roof is a piece of cardstock about 1 1/4" x 3/8" long, each side, form as a gable roof (you could use 2x6's). Cover with rice paper roofing, ragged the edges, to weather!

You could use black paper as roofing. Finish with light brown or taupe, chalk and mineral spirits. After dry (about 8 hours), give a "dash" spray of dullcote, several times to set the chalk.

Place roofing on sheathing with 5 minute epoxy.

The Light

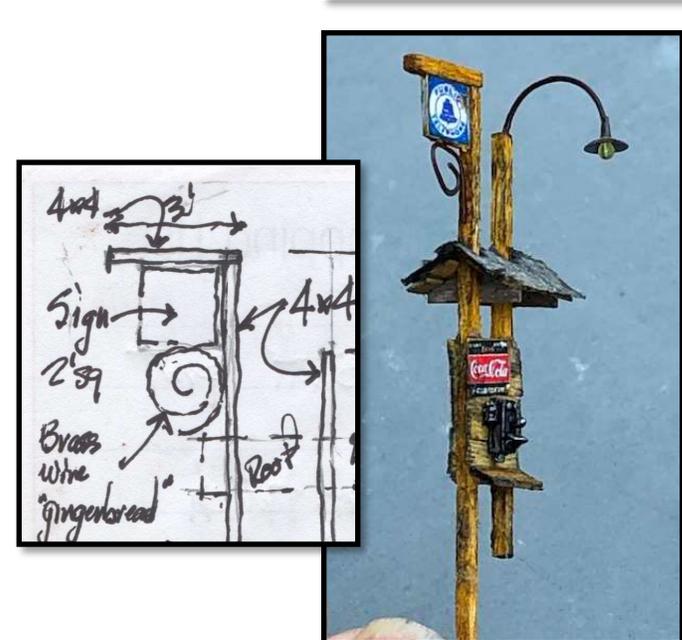
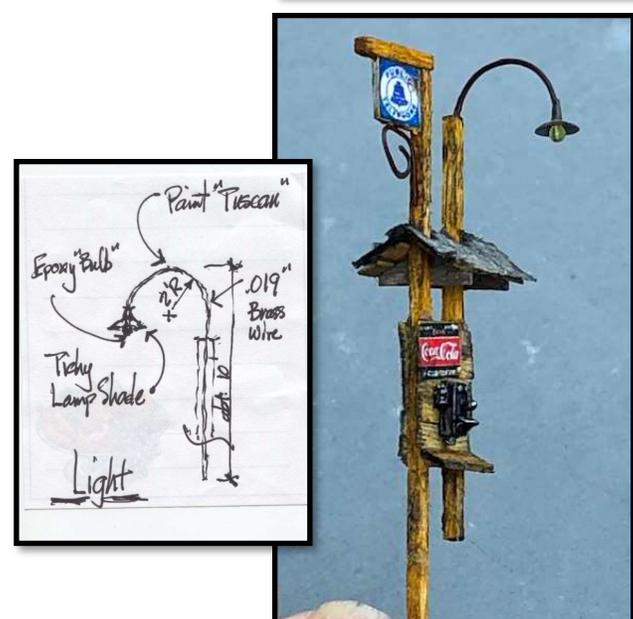
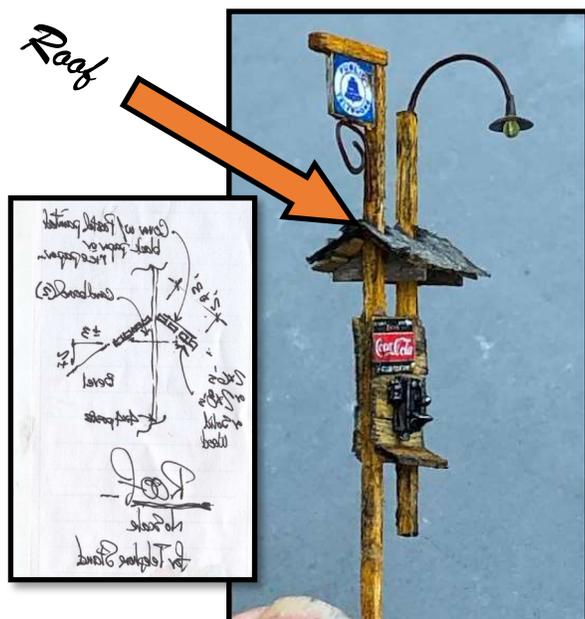
- *The shade is Tichy lampshade.*
- *Conduit is .019" brass wire, shape the arch as you desire.*
- *Lamp bulb is a small dab of 5 min epoxy. Place and dry before placing shade*
- *Paint wire and shade dark Tuscan, highlight with touches of green and black ...*

"Scrolly Gingerbread"

- + *Shape a piece of .015" or .019" brass wire ... curl to your liking!*
- + *Now ... smash on steel vice or old RR rail with hammer to flatten wire.*
- + *Install with 5 min epoxy.*

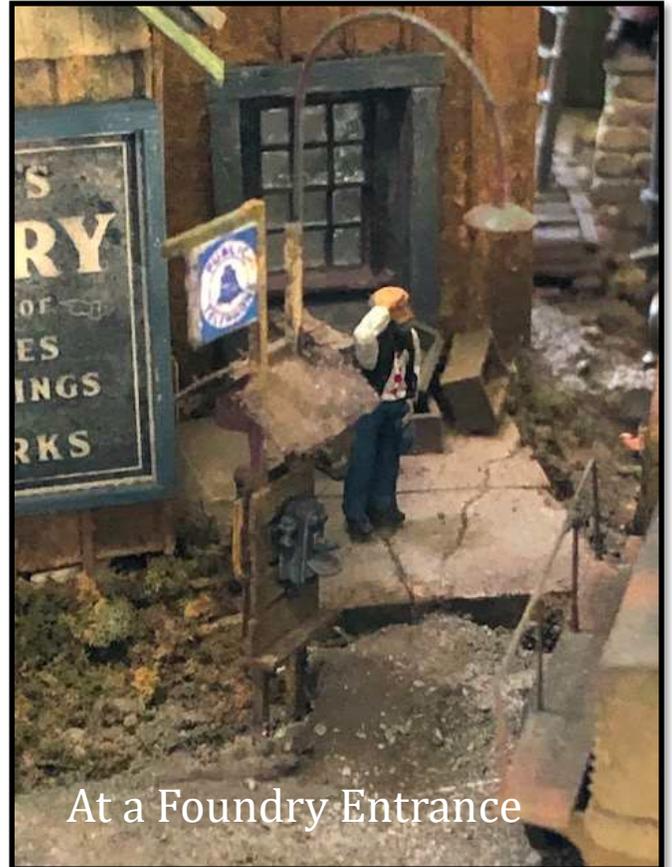
Option ...

- + *Make scroll from "quilling" paper, curl to your liking, paint and install.*



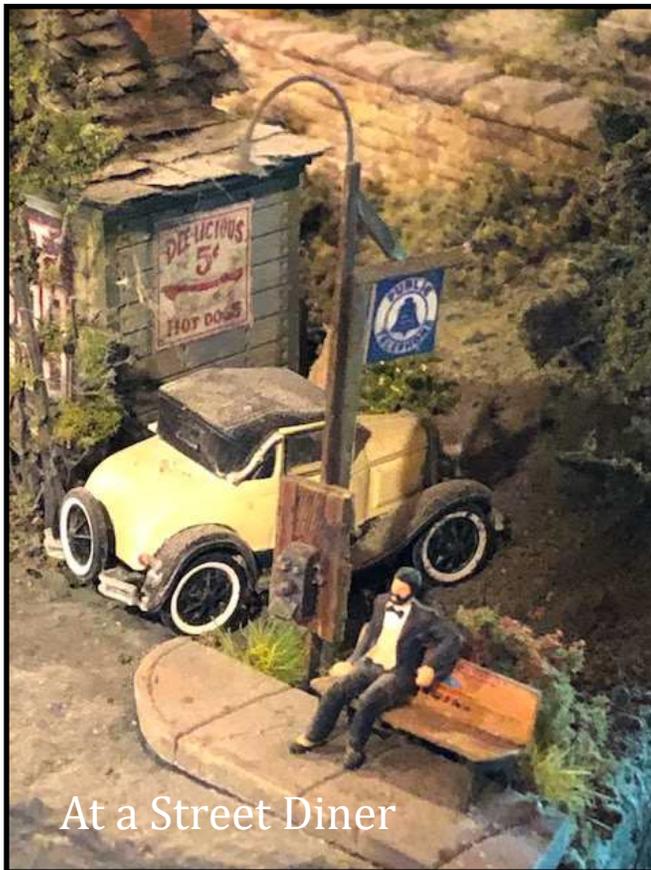


At an Icing Platform

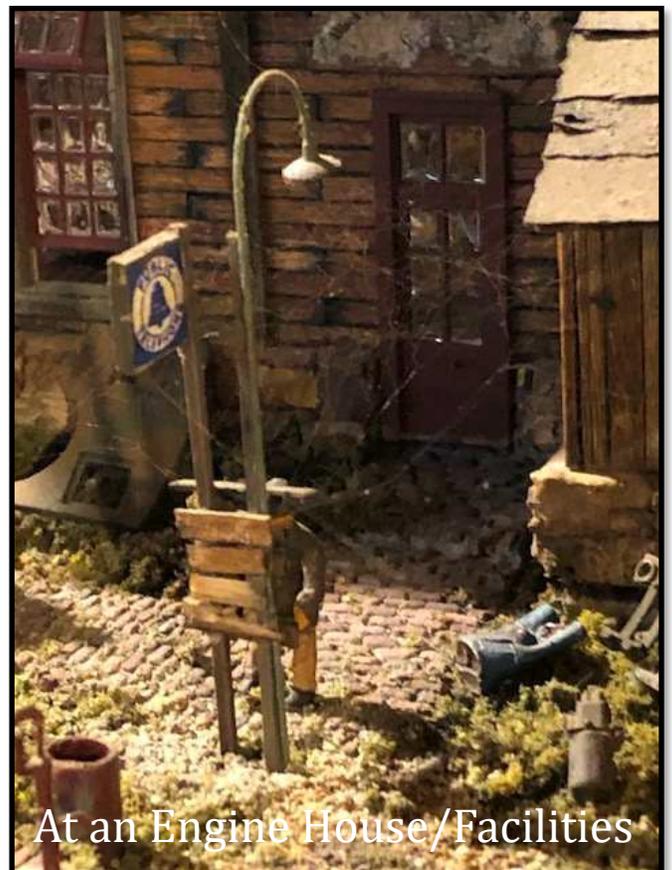


At a Foundry Entrance

Telephone Stands on Other Dioramas



At a Street Diner



At an Engine House/Facilities



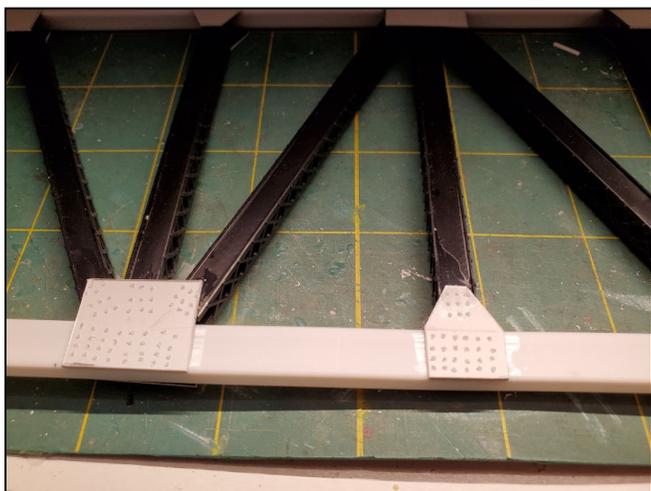
A Double Track Bridge

By Craig Drenkow, MMR

I find a lot of pleasure from scratch building things for my railroad. If you had asked me to scratch build something several years ago, I would have said “no way.” The Achievement Program requires you to build several things in different categories. After building several railcars and buildings, I took on the task of building a bridge which is a requirement for the structures category. This small Plate Girder bridge was what I built for the structures category.

So now I decided I wanted to build something new. I had a spot where the foam top of my upper deck didn't meet perfectly. I decided to build a double track bridge in the spot and put a river in the area where the seam in the foam is. I had two sections of bridge track rail from an old Walthers Double track bridge. That bridge was modeled after an older bridge that was too short for double stacks. This portion of the layout won't have a lot of double stack traffic but it could have some. I drew plans for my bridge based on the length of these two sections of track.

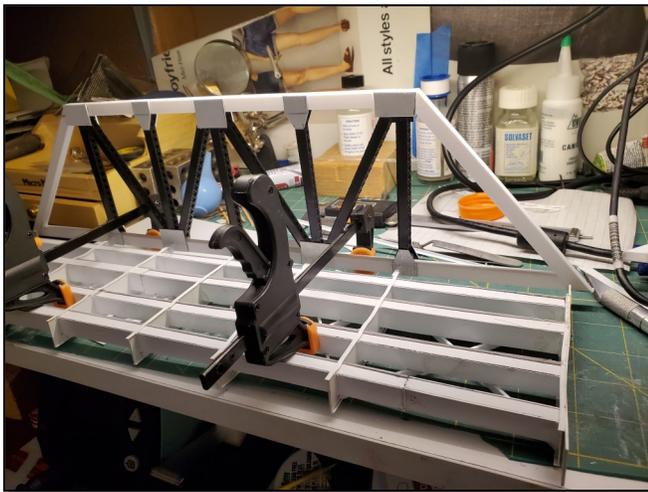
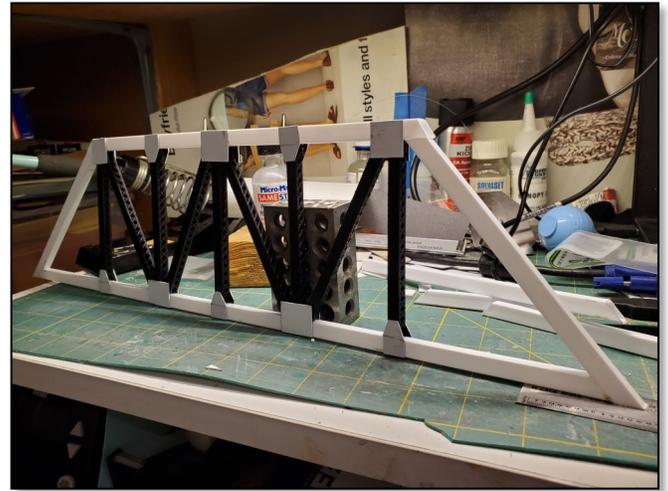
I started by building the base of the bridge. This required me to scratch build the box beams for the base. I wasn't able to find Evergreen products the size I needed so I used sheet styrene cut into strips and welded together with Plastic welder to make the beams. I created “I” beams for the cross beams from strip styrene also. The “x” bracing under the large beams is Evergreen strips. I used pictures of actual bridges from the internet to make sure my bridge looked prototypical.



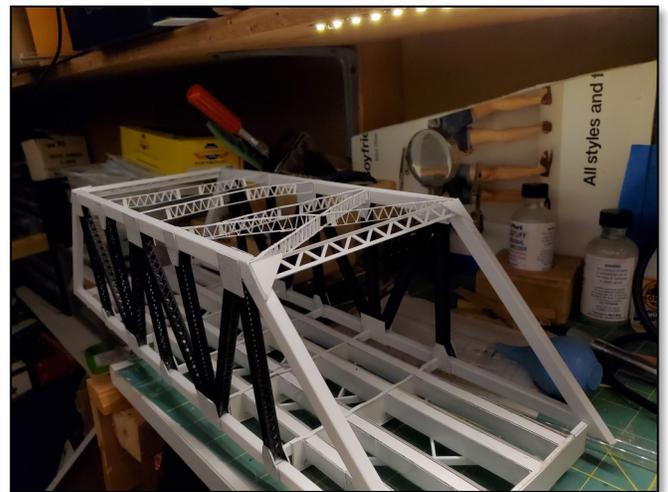
I used Evergreen Square tubing for the side trusses and the verticals and angle pieces are lattice box beams from JL Innovative. I used sheet styrene to form the plates at the joints. I used a technique where you drill a small indent into the styrene to form the rivets in the plates. Unless you are very close to the piece you can't tell that it's indented not raised and the styrene around the edges does raise up as you drill so you do feel the pattern if you run your finger across it.

I struggled a little bit with how to attach the sides of the bridge to the base. I first had to make sure the sides were flat and the tops parallel and then

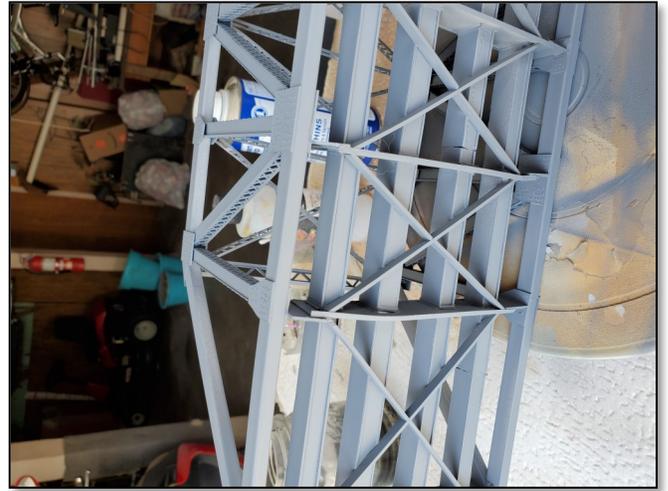
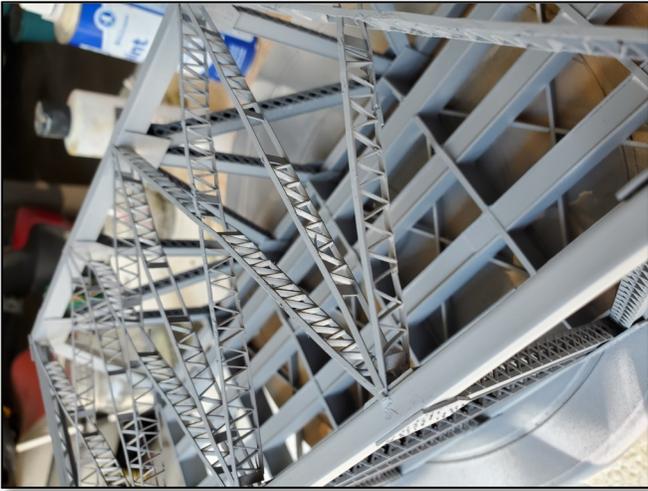
looked for pictures on the internet with good shots of the connection of the sides and base. I finally cut strips of styrene and glued them to the base and attached the sides to those pieces. I used 123 blocks to keep everything at right angles. I ended up using clamps to keep everything aligned properly while the plastic welder set up. I had to go back and reapply plastic welding solvent several times to make sure all connections were completely connected. I attached one side at a time letting it sit several days to let the plastic cure completely, not because I thought it required more time to set, but because I attached the side and it took several days to get back to the project.



The hardest part of this bridge build was the truss work across the top of the bridge. I had two pieces from Plastruc Co. that worked for the pieces on the ends of the top. I wasn't able to find pieces small enough for the center section so I ended up scratch building each one piece by piece from Evergreen Strip. It required cutting top and bottom pieces to length, and cutting multiple pieces 10 mm in length and gluing them at 45 degree angles between the top and bottom lengths. I cut wider strip to 6mm and glued them perpendicular to the top and bottom pieces. I had to build sixteen of these pieces. I spent a lot of Wednesday night chat visits building those things.



I used Stynylrez (by Badger Airbrush Co.) primer in a gray color to prime the bridge. I got very good coverage on the bridge. I then used a rust colored Vallejo paint to paint the final color. I wanted to let areas of the gray primer show through to simulate a worn off top coat of paint, but I got way better coverage than expected. I am going to have to go back and dry brush some gray paint to get the look I'm wanting. I also want to add some graffiti decals to the bridge to complete the look.



The next step will be to form the river bed under the bridge and pour water.





MCoR Member's Pike Registry

The Pike Registry is a free MCoR member benefit for layout owners that would welcome other NMRA member's visits. For more information, contact

ckeditors@mcpr-nmra.org

Indian Nations Division

Eastern Iowa Division



The Deep River Southern Railroad is a freelanced N-scale layout set in 1958. It's located in a room above the garage and is a 2-level layout designed for operations using Car Cards and Waybills. The layout is wired for DC and can accommodate 4 operators. A manual elevator connects the 2 levels. Scenery is about 90% done.

Dogwood & Redbud Shortline

The Dogwood and Redbud Shortline is my 7.5"-gauge backyard railroad that is a 550' loop with numerous bridges—two cross a creek and others are for drainage. A Super Mack, Critter, and Speeder provide the motive power and are all battery operated.

Come by, take look, go for a ride, let's operate.

Dave Salamon (918)607-2793 E-mail: drs_rr@yahoo.com



Corn Country Rails is an N-scale layout primary featuring the Rock Island Railroad in the 1950s. The layout depicts a 47 mile stretch of Rock Island's Eastern Iowa single track mainline from Homestead to Grinnell. The multi-deck layout with a helix measures 15 x 20 feet. Additional staging is in an adjacent room. It was designed for operations using four-cycle waybills, train orders, and a 3:1 fast clock. A typical operating session can keep 6-8 operators busy for about four hours with eight mainline trains, two locals, and several extras worked into the schedule. The layout is powered by an NCE DCC system with both radio and plug-in throttles. The scenery is based on rural farming towns and Iowa countryside in the summer months.

Tony Bowen, MMR – Division Superintendent

E-mail: railroadteacher@gmail.com

YouTube Channel: Corn Country Rails

Chisholm Trail Division

Central Missouri Area



Union Pacific Through the Rockies



My HO layout is a "freelanced/fictional" version of the Union Pacific in the 60's/70's on the Rio Grande's Route thru the Rockies. In a 13'x20' room, the layout is two levels connected by a 5 loop helix. The double track mainline is approximately 300 running feet with two small switching yards. NCE DCC supplies the power for the layout. The equipment roster shows UP F-Units, GP 38's and 40's, and UP/RGSD 40-2T's. Freight cars of all types supply the revenue for the line. You will see an occasional passenger train as well. 60% of the layout is landscaped and on the rest has industries/buildings in place—just not completed scenes. Industries served include coal, grain, fuel, and misc. LTL freight loads.

Terry Ross, Sup.

E-mail: terryross16@hotmail.com



Clear Creek and Quicksilver RR

The layout is a 1:20.3 indoor/outdoor layout. Outdoors has landscaping that depicts the Georgetown Loop Devil's Gate high bridge. Indoors is a logging theme railroad with basic benchwork and 250 feet of roadbed and still growing. Three locomotives have been converted to battery power with more planned.

Allen Pollock

E-mail: pollocka@mchsi.com

Western Kansas Division

Little Rock Area



Western Kansas Rails

The WKR is an N-scale, sectional layout housed in its own building (but also transportable) depicting Western Kansas. The layout measures 18x24 feet and was designed for simple operations to introduce train show spectators to the idea of operations. The third main features two staging yards, three passing sidings, and nine industries to be served powered by Digitrax DCC with radio throttles. The scenery is based on life in Western Kansas with real business names and mostly scratch-built structures, including the signature structure of the Dodge City Pride Ag Co-Op grain elevator that measures nearly 6-feet in length and dominates the skyline. Operating sessions are available.

Robert Simmons – Div. Sup.

E-mail: trainman55@hotmail.com Phone: (620) 521-3591



Missouri Pacific RR – McRae Subdivision

The fictional subdivision of the MoPac is a shelf type HO model railroad occupying a 14X20 foot room and operating with CVP EasyDCC. The railroad consists of the town of McRae, Arkansas, and the now abandoned station and the active interchange track (known as Hog Thief Crossing) with the fictional Argenta, St Joe and Northern Railroad. McRae and Hog Thief Crossing account for extensive traffic, especially during strawberry season when huge quantities of strawberries are shipped to all parts of the country. There are 5 lengthy staging tracks to supply 2 daily passenger trains, 4 freights, and 3 locals to keep operators busy running on a fast clock.

David Bogard – Div. Sup. E-mail: mopac55@hotmail.com

Western Heritage Division

Gateway Division

A freelanced version of the D&RGW Floresta Branch which ran eleven miles west of Crested Butte to a large CF&I Anthracite Mine and Breaker at Floresta, Colorado. Layout is 14'x24' point to point in On3 which I refer to as 'Poor Man's On3.' I have regularly scheduled operation sessions and welcome new operators.



Bill Grierson, Agent. Omaha, Nebraska, email: wgrierson17@gmail.com

CB&Q RR-Hannibal Division

The K line. STL--Hannibal & a branch Old Monroe to Mexico, MO are modeled on a DD deck layout 425' of main and a 75' branch in code 83 with DCC controlling 18 trains plus 5 locals & coal & Cattle trains. Featured is a 22' long Cement plant, a quarry 5' W X10'L X 3' D + large foundry & Brick plants. Plus 5 towns with fuel dealers, grain elevators and other small shipper

Hank Kraichely-Div.Sup. to contact:

Email: hkraichely@sbcglobal.net



MCoR Regional Club Rosters

This roster is created for the benefit of members of the MCoR Region. Clubs (NMRA or Non-NMRA) wanting to be listed contact ckeditors@mc-or-nmra.org

- AR, Bella Vista** **All Scales**
Sugar Creek Model Railroad & Historical Society, Inc.
PO Box #5452, Bella Vista, AR, 72714
Information: <http://sugarcreekrailroadclub.com>
- AR, Conway** **HO Scale**
Central AR Model RR Club
PO Box #1825, Conway, AR, 72033
Contact: Daniel Gladstone (501) 269-3030
- AR, Little Rock** **G, HO, N Scales**
Southwest Independent Modular Railroaders
3107 West Capitol Avenue, Little Rock, AR, 72205
- AR, Shannon Hills** **HO, N, O**
Crooked Rails Modular Club
10701 Shannon Hills Dr, Mabelvale AR 72103. Meetings 10AM Saturday. 40' x 18' traveling layout plus HO, N, and O scale at club. Visitors welcome. Contact David Ware (501-664-1923) or Charles Dempsey (510-917-5790)
- IA, Coralville** **HO Scale**
Hawkeye Model Railroad Club
860 Quarry Rd., Coralville, IA 52241. Club entrance SE corner across street from Konami Rest. Meetings: Wednesdays 7-9PM; Visitors welcome; Check website for Saturday Open House. Contact: Tom Persoon, Public Relations Officer, Persoon06@msn.com; (319)-351-0247. Website: <https://hawkeyemodelrrclub.com/> Facebook: @HawkeyeModelRailroadClub
- IA, Council Bluffs** **HO Scale**
Greater Omaha Society of Model Engineers
Contact: Brian Waters, Post Office Box 67, Council Bluffs, IA, 51502; (402) 895-0296 or (402) 491-3692
Information: SOME@TheHistoricalSociety.org
- IA, Des Moines** **HO Scale**
Central Iowa Railroad Club
Iowa State Fair Grounds
Contact: David Briely, PO Box #118, Des Moines, IA, 50301 Phone:(515) 266-8899
Information: <http://www.facebook.com/centraliowarailroadclub>
Meets: 1st Tuesday each month; Open House: 4th Friday each month.
- IA, Harlan** **N Scale**
Nishna Valley Railroad Society
1303 Eighth Street, Harlan, IA, 51537
- IA, Indianola** **HO Scale**
Warren County Modular Railroaders
Transition era. RI and CB&Q
Contact: John Averill, 14910 92nd Lane, Indianola, IA, 50125; (515) 961-3018
Iowa's only 100% NMRA club
- IL, Collinsville** **HO Scale**
Columbia Model Railroaders
410 Camelot Drive, Collinsville, IL, 62234
- IL, Glen Carbon** **HO Scale**
Metro East Model Railroad Club
180 Summit Avenue, Glen Carbon, IL
Contact: Bill Davis or Bob Gibson
email: memrrc@gmail.com
Information: www.trainweb.org/memrc
Work/run meetings 6:30PM every Thursday at Club House; Business Meetings first Thursday each month. Visitors always welcome!
- IL, Marion** **HO Scale**
Southern Illinois Train Club
PO Box 1633, Marion, IL, 62959
- KS, Augusta** **HO Scale**
Augusta Model Railroad Club, 6th & School St., 7:30. Information: info@augustahorrcub.org
- KS, Atchison**
North East Kansas Model Railroaders
12" scale, 1440 N. 6th St., Atchison, Sat. 10:00-4:00PM, Sun. 12:00-4:00PM. Information: Otto Wick 913-367-7536
- KS, Cherryvale**
Leatherock Hotel, 2nd floor, 420 N. Depot St., Cherryvale, Information: John R. Dhooghe, john@cvmrc.com or www.cvmrv.com
- KS, Cherryvale** **All Scales**
Parsons Model Railroad Engineers
Cherryvale Depot, Cherryvale, KS, 68335
- KS, Ellis** **HO Scale**
Kansas Pacific Model Railroad
Ellis Museum, 911 Washington, 10:00AM. Lunch at a restaurant afterward. Information: Tom Robinson, rrailway@gbta.net
- KS, Frankfort**
Frankfort Subdivision
416 W. 1st St., 10:00-3:00PM. Information: Joe McAtee, joem@bluevalley.net
- KS, Dodge City**
Western Kansas Rails N-Scale Layout
10594 W. Briarwood Dr., Information: Robert Simmons, 620-521-3591
- KS, Great Bend** **N, HO, O, G, Standard**
Golden Belt Model Railroad Association. Meet first Tuesday of Each Month @ 7:30PM at the First United Methodist Church, Great Bend. Correspondence to 209 E 6th, Ellinwood, KS68526, Jim Loesch,
- KS, Hutchinson** **N Scale**
Kansas Central Model Railroad Club
16 E. 3rd, Hutchinson, 11:00-4:00PM. Information: www.kansascentralmodelrailroaders.org
- KS, Lawrence**
Lawrence Model Railroad Club

- Bridge Pointe Community Church, 601 W. 20th Terrace. Information: www.lawrencemodelrailroadclub.org
- KS, Manhattan HO Scale**
Manhattan Area Rail Joiners
 Contact: Don Clagett, 1223 Pierre Street Manhattan, KS, 66502; (785) 537-7624
 email: dClagett@ksu.edu
- KS, Olathe HO Scale**
MO-KAN Rail Joiners
 Contact: Louis Seibel, 1069 North Logan Street, Olathe, KS, 66061; (913) 393-3495 or (913) 927-6850
 eMail: L-seibel@comcast.net
- KS, Overland Park O Scale**
Kansas City Module "O"
 Contact: Jack Ferris, 10334 Ash Street, Overland Park, KS 66207
 eMail: fhs1955@GMail.com
- KS, Olathe**
Weekend N-gineers
 16624 W. 126th St., Olathe, 1:00PM. Information: Ken Clark, hapheart@swbell.net
- KS, Overland Park HO Scale**
Kansas City Society of Model Engineers
 Contact: John Teeple, President, 9539 Perry Lane, Overland Park, KS, 66212; (913) 492-4142
 eMail: jsTeep@aol.com
- KS, Topeka N Scale**
Topeka N-Track Associates
 At member's home, 7:00PM. Information: Bob Wright, 785-273-7835
- KS, Topeka F/G scale**
Northeast Kansas Garden Railway Society (NEKAN-GRS)
 1308 SW Caldton Street, Topeka, KS, 66611
- KS, Wichita HO Scale**
Wichita Model Railroad Club
 PO Box #48082, Wichita, KS, 67201
 eMail: WCMR1@cs.com
- KS, Wichita N Scale**
Kansas Area N-Trak
 2046 South Elizabeth Street Apartment #1306, Wichita, KS, 67213
- KS, Wichita**
Wichita Toy Train Club
 130 S Laura, Wichita
- KS, Wichita**
Wichita Area Garden Railway Society
 At member's home, Information: Nancy Marin, nanmarin@att.net
- KS, Wichita All Scales**
Wichita Area Model RailRoders (WAMRR)
 4323 West Maple Street Wichita, KS, 67206
 Contact: Lionel A. Smith, Jr., (316) 239-1174 or (816) 518-9050; eMail: LionelSmith@hotmail.com
 Meets 2nd Thursday each month 11:30AM Spears Restaurant
- MO, Columbia HO Scale**
Columbia Area Model Railroaders (100% NMRA)
- Missouri United Methodist Church, 204 S Ninth St, or member's homes. Thursday Evenings at 6:30PM.
 Facebook: Columbia Area Model Railroaders; Contact: Marty Oetting, martyoetting@gmail.com
- MO, Jefferson City All Scales**
Capital City Model Railroaders
 PO Box #243, Jefferson City, MO, 65102-0243
 Email: pollocka@mchsi.com
- MO, Kirkwood HO Scale**
Kirkwood Railroad Association
 Meets every Thursday 7:00 - 9:00PM
 Contact: Rich Velten, 100 North Sappington Road, Kirkwood, MO, 63122
 Email: rmVelten@swbell.net
- MO, Kansas City 16" Gauge Park Train**
Kansas City Northern Miniature Railroad
 NM 60th Street & Waukonis Drive, Kansas City, MO
 Contact: W. Ohrnell (816) 746-5663
 Information: www.KCNRR.com
 Meets 1st Wednesday each month at 7:00pm
- MO, Kansas City HO Scale**
Greater Kansas City Model Railroad Club
 Contact: Walter L. Ohrnell, 6060 NW Waukomis Drive, Kansas City, MO, 64153
 eMail: wOhrnell@kc.rr.com
- MO, Kansas City HO Scale**
Southern Kansas City Model Railroad Historical Society
 8600 Ward Parkway Suite 2030 Kansas City, MO, 64114
 Contact: Richard Boone Telephone: (816) 996-1534
 eMail: rBoone@traintown-kc.com
 Meets 2nd Monday each month 7PM Open house Sat. & Sun. 12 to 5PM
- MO, Kansas City N Scale**
Weekend En-gineers
 8600 Ward Parkway, Kansas City, MO, 64114
 Contact: Richard Boone: (816) 966-1534
 eMail: rBoone@traintown-kc.com
 Meets 3rd Sunday at 11:30AM
- MO, Kansas City**
Kansas City Narrow Gaugers
 Members Homes, Information: Dean Windsor, On3@worldnet.att.net
- MO, Kansas City Standard, G, O, S, HO Marklin, HO, N, Z Scales and Wooden Trains**
Union Station Kansas City Model Railroad Society
 30 West Pershing Road, Kansas City, MO, 64101
 Contact: Ted Tschirhart, Telephone: (816) 816-3449
 eMail: TedTtschi@kc.rr.com
- MO, Liberty**
Heartland N-Trak Of Greater Kansas City
 131 S. Water St., Liberty, MO, 1:00. Information: Bob Osborn, 816-452-9227
www.lawrencemodelrailroadclub.org
- MO, North Kansas City N Scale**
Missouri Northern Railroad Society, Inc.
 PO Box #12591
 North Kansas City, MO, 64116
- MO, Odessa HO Scale**

Eastern Jackson County Mainliners Model Railroad Club
"Outlet Mall", Odessa, MO, 64076
Information:
www.EasternJacksonCountyMainlines.com

MO, Saint Louis N Scale

Mississippi Valley N Scalers
1684 Harbor Mill Dr., Fenton, MO, 63026
eMail: mvns@railfan.net
Website: <http://mvns.railfan.net>
Facebook: <https://www.facebook.com/mvnsrr>
Featuring NTRAK and T-TRAK modular layouts displayed at regional shows.

MO, Saint Peters HO_{n3} Scale

Modular HO Narrow Gauge Society
914 Summer Leaf Drive, Saint Peters, MO, 63376

MO, Savannah G, O, HO Scales

Green Valley Baptist Model Railroad Club
11993 County Road 162, Savannah, MO, 64485
Contact: Nancy Adams (816) 262-0304
eMail: GreenPetticoat@yahoo.com

MO, Springfield HO Scale

Ozark Model Railroad Association
424 West Commercial Street, Springfield, MO, 65803,
Info: <http://www.omraspringfield.org/contact.html>

MO, Webster Groves 2-Rail O Scale (1/4" to the foot)

Big Bend Railroad Club, Inc.
8833 Big Bend Blvd., Webster Groves, MO, 63119
Email: secretary@BigBendRRclub.org
Information: www.bigbendrrclub.org

NE, Fremont All Scales

Nebraska Railroad Museum
1835 North Somers Avenue, Fremont, NE, 68025
Contact: Dave Fachman (402) 727-0615
eMail: fevr@FremontRailroad.com
Information: <http://www.FremontRailroad.com>

NE, Hastings N Scale

Tri-City Model Railroad Association
607 South Shore Drive, Hastings, NE, 68901

OK, Claremore All Scales

Claremore & Southern



Region Resources

Online Division Libraries in the Region



Waiting at the Station

Our NMRA Divisions offer numerous articles and clinic presentations in their online libraries. Check out their offerings here:

Gateway Division

<http://www.gatewaynmra.org/model-railroad-article-library/>

Turkey Creek Division

<http://www.tc-nmra.org/TC-Library.html>

Indian Nations Division

<http://www.tulsanmra.org> and click on "Resources"

In the Blue Mountains of Oregon



Ray Brady Photo